



CITY OF BAYPORT

294 NORTH 3RD STREET
BAYPORT, MN 55003

PLANNING COMMISSION MEETING

City Hall - Council Chambers

March 16, 2015 – 6:00 p.m.

A. CALL TO ORDER

B. APPROVAL OF MINUTES

- **October 20, 2014 regular meeting**

C. PUBLIC HEARINGS

- **Public hearing to consider a Planned Unit Development (PUD) General Development Plan and a Preliminary Plat for Inspiration Phase III-B**
- **Public hearing to consider a variance to allow an attached garage and addition to the existing single-family home on the property located at 207 6th Street North**

D. OLD BUSINESS

E. NEW BUSINESS

F. GENERAL INFORMATION

G. OPEN FORUM

H. ADJOURNMENT

**CITY OF BAYPORT
PLANNING COMMISSION MEETING MINUTES
CITY COUNCIL CHAMBERS
OCTOBER 20, 2014
6:00 P.M.**

CALL TO ORDER

Pursuant to due call and notice thereof, Commissioner Ritzer called the regular Bayport Planning Commission meeting of October 20, 2014, to order at 6:00 p.m.

ROLL CALL

Commissioners Present: Brad Abrahamson, Beth Kelly, Jeff Richtman and Joe Ritzer

Commissioners Absent: None

City Staff Present: Assistant City Administrator/Planner Sara Taylor and City Council Liaison Patrick McGann

APPROVAL OF MINUTES

It was moved by Commissioner Richtman and seconded by Commissioner Abrahamson to approve the April 21, 2014 meeting minutes as presented. Motion carried.

PUBLIC HEARINGS

Consider a request for a minor subdivision to create two separate parcels at 220 1st Avenue South, together with a variance from the side yard setback requirement for the existing single-family house on the property: Planner Taylor reviewed an application to subdivide the property at 220 1st Avenue South to create two separate parcels and allow a side yard setback variance of 2.4 feet. The property was originally platted as three separate lots; however the lots were combined into one parcel many years ago for tax purposes. At this time, the applicant would like to conform with current zoning standards and subdivide the property into two lots, one to include the existing single-family home and a second lot for the future construction of a new single-family home. As a result of the proposed minor subdivision, the existing home will be set back 7.6 feet from the new east side lot line, which does not comply with the required 10 foot setback. Therefore, a 2.4 foot setback is required to comply with current zoning code. Planner Taylor noted the property is large enough to divide into two standard single-family lots; however due to the orientation of the existing home on the property, a variance from the east side yard setback is required. The setbacks for the existing home will be similar to adjacent properties in this residential zoning district and not have a negative impact on the neighborhood. Staff recommended approval of the minor subdivision and variance, subject to the conditions stated in the staff report. Notice of tonight's public hearing was published in the Stillwater Gazette and mailed to all property owners within 350 feet of the subject property. No public comment was received related to the application. Planner Taylor said a new home on the second parcel could be situated on the lot to best fit the developer's plan and would not be limited to access off 1st Avenue South.

Commissioner Ritzer opened the public hearing and no comments were heard.

It was moved by Commissioner Richtman and seconded by Commissioner Kelly to close the public hearing. Motion carried.

It was moved by Commissioner Kelly and seconded by Commissioner Abrahamson to recommend to the City Council to approve a minor subdivision to create two separate parcels at 220 1st Avenue South, together with a 2.4 foot variance from the east side yard setback for the existing single-family house on the property, subject to the findings of fact and conditions of approval stated in the staff report. Motion carried.

The application will be heard by the City Council on November 10, 2014.

OLD BUSINESS – None

NEW BUSINESS – None

GENERAL INFORMATION

Update on a concept plan for a multi-family residential building for Phase II of the Inspiration development: Planner Taylor reported that earlier this month the City Council reviewed a concept plan for Phase II of Inspiration in order to provide preliminary input to the applicant on the proposed plan and give the City Council an opportunity to ask questions. Since that meeting, staff has not heard whether the applicant intends to proceed with the concept plan; however Planner Taylor encouraged the commissioners to review the proposal presented at the October 6 City Council meeting, as well as the meeting minutes posted on the city’s website. If the developer proceeds with a formal final plat application, the application would be considered by the Planning Commission for a recommendation to the City Council.

Resignation of Planning Commissioner Brad Hallett: Planner Taylor reported that Commissioner Hallett recently submitted his resignation. Over the next few months, staff will be soliciting applicants to complete his term ending December 31, 2015. Interested residents should contact City Hall for more information. Commissioner Hallett was recognized for his service on the Planning Commission.

OPEN FORUM – None

ADJOURN

It was moved by Commissioner Abrahamson and seconded by Commissioner Richtman to adjourn the meeting at 6:10 p.m. Motion carried.



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PLANNING REPORT

TO: Bayport Planning Commission

FROM: D. Daniel Licht, AICP

RE: Bayport – Inspiration Phase III-B

REPORT DATE: 12 March 2015

60-DAY DATE: 18 April 2015

TPC FILE: 195.02 – 15.01

A. BACKGROUND

Mattamy Homes, Inc, on behalf of owner Inspiration Holdings, LLC has submitted application for a Planned Unit Development (PUD) general development plan and preliminary plat for Outlot A, Inspiration. The Inspiration development was originally approved by the City Council in October of 2004 as a PUD general development plan and preliminary plat that included a total of 328 residential housing units and 145 acres of restored native prairie open space. There are 137 single family lots that have been final platted and developed within Phase I and Phase III-A.

The PUD general development plan and preliminary plat for Phase III-B submitted by Mattamy Homes proposes revisions to the approved subdivision design and PUD standards for this portion of Inspiration to develop 113 single family lots and provides 20.15 acres of outlots to be set aside as permanent open space. The approvals for Inspiration in 2004 are the framework for which the City Council is to consider the current request to amend the PUD development stage plan and preliminary plat to verify that the application complies with the zoning and land use intended for the development and is consistent with, and compliments the standards approved as part of Phase I of the development.

The Planning Commission and City Council reviewed the proposed Phase III-B Development as a concept plan at a joint meeting on 20 January 2015. The concept plan review meeting included presentations by representatives of the Inspiration Homeowner’s Association (HOA), Architectural Review Committee (ARC), and Mattamy Homes. Based on the comments at the

concept plan review meeting, Mattamy Homes proceeded to prepare detailed plans and submit an application to amend the PUD general development plan and preliminary plat for Inspiration Phase III-B. A public hearing has been noticed for the Planning Commission meeting on 16 March 2015 to consider the application.

Inspiration Holdings, LLC also held multiple neighborhood meetings over the past year to help existing Inspiration residents understand the development covenants, architectural standards, homeowner's association, obligations of the developer related to the continued build out of Inspiration, as well as the current financial status of the development. These meetings included presentation and discussion of the proposed Mattamy Homes concept plan. According to the property owner, no opposition has been expressed by Inspiration residents in regard to the concept plan or Mattamy Homes' architectural portfolio for the single family homes. Inspiration Holdings, LLC has also appointed two existing Inspiration residents to serve on the HOA and ARC. The outreach by the property owner to conduct the neighborhood meetings and appoint existing Inspiration residents to the ARC has helped to foster a partnership between the owner/developer and Inspiration residents by allowing the residents opportunities for input and participation in decision making affecting the neighborhood.

Exhibits:

- Mattamy Project Narrative
- Letter from Maxfield Research, Inc. dated 3/10/2014
- Inspiration 3rd Addition plan set dated 2/13/2015 (13 sheets)
- Graphic with 2004 and 2015 Site Plans Compared Side-By-Side
- Mattamy architectural renderings
- City Engineer's Report dated 3/10/2015
- Washington County plan review comments dated 3/11/2015

B. ANALYSIS

Comprehensive Plan. The Comprehensive Plan guides development of the subject site for a mix of residential land uses and natural features based on the previously approved PUD general development plan and preliminary plat for Inspiration approved in 2004. The single family lots and open space land uses proposed for Inspiration Phase III-B are consistent with the intent of the City's Comprehensive Plan for future land use development of the subject site.

Zoning. The subject site is zoned R- PUD District with a base zoning classification of R-2, Single Family Urban District and permitted uses as set forth in Section 6 of the Zoning Ordinance. The applicant is proposing to develop single family lots and open space in accordance with the amended PUD general development plan and preliminary plat. Development of single family lots as a permitted use and open spaces complies with the PUD District. Specific changes being proposed to the subdivision design and lot requirements will be outlined in subsequent paragraphs. A PUD permit will be drafted upon approval of the PUD general development plan

and preliminary plat establishing the uses, lot requirements, setbacks and other stipulations of said approvals for consideration with application for final plat and recorded with the parcels within the development.

Development Plan. The following is a summary of the changes from the original 2004 preliminary plat as shown on the revised PUD general development plan and preliminary plat submitted by Mattamy Homes:

- The overall number of single family lots within the area of Phase III-B has been decreased by three lots.
- The area bounded by Inspiration Parkway North, Primrose Path, Planters Path and Prairie Way has been extensively revised. The planned private alleyways within the village lot area have been eliminated and instead Periwinkle Place is provided as a looped public street off of Planters Path to access 10 lots. The applicant has submitted market research information prepared by Maxfield Research, Inc., which is attached for reference, detailing market trends away from small lots accessed by private alleys and provides justification for the change in the site design.

At the center of the Periwinkle Place loop street is Outlot B, which is to be developed as a village green for open play with a paver seating and fire pit area. On the outside of the lots accessed from Periwinkle Place is Outlot C that will be developed with native plantings and trees, although the area will primarily function as areas for storm water management basins. A trail between the village green outlot and Inspiration Parkway North will be provided to allow pedestrian access from south portions of the neighborhood through the block to access Barker’s Alps Park. Maintenance of this trail will be an HOA responsibility along with the improvements in the village green.

- The amended PUD general development plan and preliminary plat maintains a landscaped roundabout at the intersection of Inspiration Parkway North and Primrose Path consistent with the original 2004 site plan. The design of the roundabout is subject to City Engineer approval to ensure adequate turning movements for vehicles navigating the intersection.
- The revised site plan includes additional sidewalks along the south side of Inspiration Parkway North west of Primrose Path, along Primrose Path north of Inspiration Parkway and at the perimeter of the roundabout. No sidewalk is provided on Periwinkle Place for the lots across from the village green.
- The proposed Phase III-B subdivision design includes eight lots on the south side of Inspiration Parkway between Stagecoach Trail (CR 21) and Primrose Path. This area was approved to be a landscaped passive open space within Outlot C with the 2004 PUD general development plan and preliminary plat.

- Mattamy Homes has submitted its architectural plan and exterior material portfolio for the homes to be built in Phase III-B to the development’s Architectural Review Committee. The ARC, which includes homeowner representatives, has given preliminary approval to Mattamy Homes’ house plans.

Lot Requirements. Inspiration was approved with unique lot requirements that deviated from the standard requirements of the City’s R-2, Single Family Urban Zoning District. The developer is proposing a single set of minimum lot requirements that are larger than the village lots and smaller than the traditional lots included in Phase I. Within these minimum standards there are two different lot types provided at 65 feet wide and 75 feet wide to accommodate a range of house styles within Mattamy Homes’ portfolio. Given the context of the overall PUD general development plan and preliminary plat that includes permanent open space and the ability of these lots to accommodate a range of house styles and sizes, the proposed lot requirements are consistent with the intent of the flexibility from the R-2 District standards approved in 2004.

	R-2 District	Inspiration (2004)		Phase III-B
		Village	Traditional	
Min. Lot Area	10,000sf.	5,500sf.	9,600sf.	7,668sf.
Min. Lot Width	75ft.	50ft.	80ft.	65ft.
Min. Lot Depth	130ft.	100ft.	100ft.	125ft.
Max. Impervious Lot Coverage	35%	62%	48%	48%

Setbacks. The Inspiration PUD District was approved with setbacks that deviated from the requirements of the R-2 Zoning District, particularly for the small village lots, some of which had alley access, as shown below in the table below. Except for the side yard setback, the requirements proposed for the amended PUD general development plan and preliminary plat are consistent with the setbacks of the R-2 District. Requiring a 7.5 foot setback from interior side lot lines regardless of whether the building is part of the dwelling or part of an attached garage simplifies construction upon each lot and ensures a minimum 15 foot separation between principal buildings for rear yard access and fire safety. City staff considers the proposed setbacks to be appropriate given the context of the 2004 approvals and the demonstrated ability to construct a variety of house styles and sizes upon these lots.

	R-2 District	Inspiration (2004)		Phase III-B
		Village	Traditional	
Front	20ft.	15ft.	20ft.	20ft.
Side Corner	20ft.	20ft.	20ft.	20ft.
Interior	10ft./House 5ft./Garage	10ft./House 5ft./Garage Total 15ft.	10ft./House 5ft./Garage Total 15ft.	7.5ft.
Rear	30ft.	23ft./House and 5ft./Garage	20ft.	20ft.

Open Space. The revised site plan includes seven outlots with a preliminary total area of 24.03 acres. Outlots A, C, D, E, F and G are to be deeded to the City, preserved through a

conservation easement and held by Minnesota Land Trust, and maintained by the Inspiration Stewardship Foundation consistent with the 2004 Inspiration PUD general development plan and preliminary plat approvals. Outlot B will be retained by the HOA as private open space. The plans for Outlot B include development of a paver seating area with a fire pit and large turf grass open space for recreation. An easement for the private trail within Outlot C will need to be recorded with the final plat approval to provide for maintenance.

Landscaping. The revised PUD general development plan and preliminary plat submittal includes a detailed landscape plan for Phase III-B. The landscaping is consistent with the types and quantities shown on the original 2004 PUD general development plan as well as the amended open space restoration plan approved in 2012. Mattamy Homes will install the plantings as shown on the landscape plan with Inspiration Holdings LLC responsible for the prairie restoration seeding in Outlot D and F, which must be completed prior to the City accepting the improvements.

Access. The subject site will have access via Stagecoach Trail (CR 21) with an intersection at Inspiration Parkway North, the location of which is the same as approved in 2004, as well as connections to existing local streets within Inspiration Phase I. The design and access of the north intersection of Inspiration Parkway and Stagecoach Trail (CR 21) is subject to Washington County approval. Washington County is requiring that this intersection be designed to allow only three-quarter turning movements prohibiting a left turn from Inspiration Parkway North to southbound Stagecoach Trail. This requirement by Washington County is being made in conjunction with the installation of a traffic signal at Inspiration Parkway South and Stagecoach Trail, which Washington County believes is warranted based on the school development west of the roadway and increasing traffic on Stagecoach Trail (CR 21). City staff has had extensive discussions with Washington County staff to maintain at least three-quarter turning movements at Inspiration Parkway North and Stagecoach Trail (CR 21) for neighborhood access. With this type of intersection, the City staff finds that there will be no negative traffic impacts from the Phase III-B development within the neighborhood as:

- There are a limited number of homes within Inspiration (and three fewer than originally approved per the revised site plan) and the potential traffic volume on the streets within the neighborhood would not exceed the design capacity of a local residential street.
- Some portion of the traffic generated within the neighborhood can still be expected to utilize the intersection at Inspiration Parkway North to access northbound Stagecoach Trail (CR 21).
- Placement of a signal at Stagecoach Trail (CR 21) and Inspiration Parkway North may entice more traffic to utilize that intersection to access southbound Stagecoach Trail (CR 21) even if the turning movements at Inspiration Parkway North were not restricted.

Additional comments on access and traffic are provided in the City Engineer's Report, which is included as an attachment.

Streets. The Inspiration PUD general development plan and preliminary plat approved in 2004 allowed for platting of 50 foot wide rights-of-way for public streets. The streets were to be constructed as 26 feet wide with an urban section curb and gutter on one side and rural section ditch on the other as part of the planned drainage train within the development. The developer is proposing to utilize the same street sections as part of Phase III-B. The 26 foot wide streets are to be posted no parking on the traditional curb side of the street to ensure adequate width for emergency vehicles at all times. Primrose Path in the area of Lots 10-13, Block 10 is to be revised to eliminate the eyebrow street section and island and provide a 30 foot wide corridor to Outlot D. The developer shall submit a plan for the installation of street signs. All street designs, street construction and street signs plans are subject to review and approval by the City Engineer.

Trails/Sidewalks. A five foot wide concrete sidewalk is to be constructed along one side of streets having an urban section design. There is no sidewalk along either side of Periwinkle Place adjacent to the village green and City staff recommends that a sidewalk be added on the outside of the street abutting the lots within Blocks 3 and 4 and Outlot C. The trail plan for provision of eight foot wide asphalt off-street trails through open space areas approved in 2004 with the original Inspiration PUD general development plan and preliminary plat will be completed as part of Phase III-B. The section of sidewalk along Inspiration Park Way North from the trail in Outlot G to the west side of Lot 8, Block 13 should be replaced with trail for City maintenance purposes. The combination of sidewalks and trails within Inspiration Phase III-B is consistent with the 2004 Inspiration PUD general development plan and preliminary plat and will provide excellent pedestrian, bicycle and recreational user circulation within Inspiration and access to/from the neighborhood via the existing trail to Barker's Alps Park.

Grading Plan. The developer has submitted a grading plan and storm water runoff calculations for the proposed preliminary plat. The submitted plans identify a stand of trees within Outlot A to be preserved and a second stand within the area of Blocks 12, 13 and Outlot F that will be removed. The plans approved in 2004 for Inspiration included handling storm water drainage using a combination of curb and gutter, storm sewer and overland through drainage train. The proposed grading plan would utilize urban section streets with curb and gutter and storm sewer to a greater extent to manage storm water within the development. All grading, drainage and erosion control plans and issues are subject to review and approval by the City Engineer. Wetland issues are additionally subject to review and approval by Washington County and the Middle St. Croix Watershed District.

Utility Plan. The developer has submitted plans for extension of sanitary sewer, water and storm sewer within the preliminary plat. The development of Phase III-B will allow second connection of the water mains within Inspiration to trunk facilities within Stagecoach Trail (CR 21) to loop the system providing improved water quality and ensuring service. The utility plan must be revised to identify the location of proposed street lights within Inspiration Phase III-B,

which were to be installed only at street intersections and trail entrances. All sanitary sewer, water main, storm sewer and street light plans are subject to review and approval by the City Engineer.

Easements. The preliminary plat illustrates 10 foot wide drainage and utility easements at the perimeter of all of the single family lots (overlying the side lot lines five feet each side) and over all of Outlots A, C, D, E, F and G. Outlot B has a drainage and utility easement only at the perimeter of the parcel to allow for development of the village green. All easements are subject to review and approval of the City Engineer in accordance with Section 7.7 of the Subdivision Ordinance.

Phasing Plan. The developer must submit a phasing plan for development of the lots and construction of streets and utilities with application for final plat approval. The phasing plan is subject to review by City staff and approval of the City Council.

Park Dedication. Development of Phase III-B remains subject to the Inspiration Master Development Agreement entered into upon approval of the original final plat for Phase I on April 4, 2005. The Master Development Agreement requires that subsequent final plats of Inspiration after Phase I would satisfy park dedication requirements established in Section 8 of the Subdivision Ordinance through payment of a cash fee in lieu of land equal to 10 percent of the fair market value of the subject site. The fair market value of the subject site is to be determined by an appraiser agreed to by the developer and the City at the time of application for final plat approval.

The original development plan for Inspiration includes a parking area for the southern portion of Barker's Alps Park accessible from Inspiration Parkway North. In 2009, the City did a master plan for Barker's Alps Park and improvement project that included play area, ball field and trail improvements as well as a temporary gravel parking lot within the future right-of-way for Inspiration Parkway North. The temporary gravel parking area will need to be removed for construction of Inspiration Parkway North with Phase III-B. The plans for Phase III-B will also need to be revised to provide for on-street parallel parking stalls along Inspiration Parkway North and connection of the existing trail within the area of Outlot D consistent with the original Inspiration development plan.

Development Agreement and PUD Permit. The developer will be required to enter into a separate development agreement and PUD permit specifically for Inspiration Phase III-B as a condition of approval for a subsequent application for final plat. The City Attorney will coordinate execution of these documents upon final plat approval by the City.

Burial Mounds. Three burial mounds were identified within the area of the Inspiration development and were to be protected by being established within Outlot N, Inspiration approved with the Phase I final plat. No portion of Outlot N, Inspiration is being affected by the proposed Phase III-B development, so there will be no negative impacts to the burial mounds.

The obligations of the property owner related to the burial mounds remain enforceable under the Phase I development agreement and are not a subject of the current application.

Nature Center. The 2004 PUD general development plan approval included a requirement for the original developer to convey the nature center to the City for public use, upon completion of the open space restoration. The conveyance of the nature center hasn't occurred to date. The open space restoration is anticipated to be completed in 2015.

Outlot M, Inspiration. The final plat of Inspiration Phase I included Outlot M at the southwest corner of the development. This parcel is separated from the balance of the Inspiration development by railroad right-of-way and has sufficient area to be a buildable parcel. The Master Development Agreement for Inspiration Phase I provides that the City be given first opportunity to acquire the parcel for future public use. No action regarding Outlot M, Inspiration is contemplated as part of the current Phase III-B application.

C. CITY STAFF RECOMMENDATION

The amended PUD general development plan and preliminary plat as proposed by Mattamy Homes retains the key components intended for the Inspiration development, while reflecting current market trends and preferences, and is consistent with the original performance standards adopted in 2004. Our office and City staff recommends approval of the amended PUD general development plan and preliminary plat for Inspiration Phase III-B subject to the conditions outlined in Section "D" below.

D. SUGGESTED FINDINGS OF FACT AND CONDITIONS OF APPROVAL

The Planning Commission is asked to consider approval of the Inspiration Phase III-B PUD general development plan and preliminary plat subject to the following stipulations:

1. Issuance of a building permit for construction of a single family home within Phase III-B shall be contingent upon certification from the Inspiration Architecture Review Committee that the proposed structure complies with the Inspiration architectural guidelines.
2. Single family lots shall be subject to the following lot and setback requirements:

Lot Requirements:	
Min. Lot Area	7,668sf.
Min. Lot Width	65ft.
Min. Lot Depth	100ft.
Max. Impervious Lot Coverage	48%
Setbacks:	
Front	20ft.

Side Corner	20ft.
Interior	7.5ft.
Rear	20ft.

3. Outlots A, C-G as shown on the preliminary plat shall be deeded to the City, preserved through a conservation easement and held by Minnesota Land Trust and maintained by the Inspiration Stewardship Foundation consistent with the 2004 Inspiration PUD general development plan and preliminary plat approvals; the property owner shall complete the landscaping and restoration plan for Outlots A, C-G prior to the City assuming responsibility for the maintenance of the outlots as has been agreed.
4. The builder shall provide a cash escrow with application for each building permit guaranteeing installation of two street/yard trees to be installed after home construction is complete.
5. Construction of the north intersection of Inspiration Parkway and Stagecoach Trail (CR 21) is subject to Washington County approval and the developer will be required to reimburse the City for all costs related to obtaining the required access and right-of-way permits.
6. One side of all public streets shall be designated as no parking zones and the developer shall be required to install no parking signage as deemed necessary.
7. Primrose Path in the area of Lots 10-13, Block 10 is to be revised to eliminate the eyebrow street section and island.
8. The submitted plans are revised such that the section of sidewalk along Inspiration Park Way North from the trail in Outlot G to the west side of Lot 8, Block 13.
9. All street designs, street construction and all street signs are subject to review and approval by the City Engineer.
10. The submitted plans shall be revised to provide an urban street section and sidewalk along the side of Primrose Place abutting Block 3, Block 4 and Outlot C, subject to review and approval of the City Engineer.
11. All grading, drainage and erosion control plans and issues are subject to review and approval by the City Engineer with wetland issues additionally subject to review and approval by Washington County and the Middle St. Croix Watershed Management Organization.
12. All sanitary sewer, water main, storm sewer and street light plans are subject to review and approval by the City Engineer.

13. All easements are subject to review and approval of the City Engineer.
 14. Satisfaction of park dedication requirements established in Section 8 of the Subdivision Ordinance shall be through payment of a cash fee in lieu of at the time of final plat approval and calculated in accordance with the terms of the Master Development Agreement, with a credit for the cost of on-street parking stalls constructed by the developer adjacent to Outlot D.
 15. The developer will be required to enter into a separate developer's agreement and PUD permit specifically for Inspiration Phase III-B as a condition of approval for a subsequent application for final plat.
 16. The developer shall submit a complete application for final plat in accordance with the approved phasing plan no longer than one year after said approval, or approval of the preliminary plat shall be considered void, unless a request for time extension is submitted in writing by the applicant and approved by the City Council in accordance with Section 4-2-1.16 of the Subdivision Ordinance.
- c. Logan Martin, City Administrator
Sara Taylor, Assistant City Administrator/City Planner
Andy Pratt, City Attorney
John Parotti, City Engineer
Rick Packer, Mattamy Homes, Inc.
Terry Forbord, Inspiration Holdings, LLC



Project Narrative

Inspiration 3rd Addition

Bayport, Minnesota

History

Inspiration was originally proposed as a conservation development in 2004 by Homer Thompkins of CPDC. The principal guiding design element was the clustering of home sites which allowed the preservation of significant open spaces and ecosystems. A victim of the real estate crash in 2008, the development has undergone several ownership changes and development attempts in the last six years. The project is presently under the ownership of Inspiration Holdings, LLC and under contract with Mattamy (Minneapolis) Partnership (Mattamy Homes).

Existing Conditions

The portion of Inspiration being proposed for development by Mattamy has no buildings on the site and has, to some extent, been disturbed by previous development. There are some scattered trees along the north boundary of the site; these are being preserved. The "Prairie Area" is not being purchased by Mattamy and is currently undergoing restoration efforts by Inspiration Holdings. All other significant historical landscapes and features have been identified and preserved during the original development process.

Applicant

The applicant is Mattamy Homes or Mattamy (Minneapolis) Partnership. Mattamy is a privately owned company based in Toronto, Canada where it is Canada's largest homebuilder. Mattamy (Minneapolis) is Minnesota's largest privately held builder its 5th largest builder overall. We have projects and/or large holdings in Minnetrista, Medina, Lakeville, Lino Lakes, Otsego, Waconia, Victoria, North Oaks, Carver, Maple Grove, and Blaine. As mentioned above, Inspirations Holdings, who's managing partner is Terry Forbord is currently the owner of the property.

Development Proposal

Mattamy is proposing to develop the remaining portion of Outlot A with 113 detached single family homes with eighty 65' and thirty-three 75' wide lots. The plan respects the

principal intent of Inspiration in that it preserves the open spaces originally proposed and keeps home sites in roughly the same areas. A private neighborhood park is proposed (and will be built by the Mattamy) for use by the residents of Inspiration. An important aspect of the proposal is that it will provide an additional access to Stagecoach Trail, minimizing congestion at its intersection with Inspiration Parkway and providing for added fire and safety vehicle access to the neighborhood.

Original Inspiration Proposal

Mattamy's proposal is reflective not only of the conservation development theme of Inspiration but also in the way that it responds to the site themes. Similarities between the original proposal for Inspiration and the Mattamy proposal are as follows:

1. Very similar amounts of open space and areas devoted to home sites.
2. Road patterns and traffic flow, including proposed round-about.
3. Landscaping
4. Storm water treatment
5. Open space improvements
6. Density

Differences between the original proposal for Inspiration are as follows:

1. Mattamy proposal has 3 fewer lots.
2. Mattamy proposal emphasizes views to open spaces from public streets. CPDC contained open spaces that could not be seen and limited viewsheds, restricted pedestrian access from many streets, and made the streetscape seem more contained.
3. The Mattamy proposal provides space for a neighborhood park devoted to open space games and neighborhood gatherings. The entire space is useable; none of it is devoted to stormwater treatment.
4. Elimination of the "alley loaded" home offerings. I have included a brief white paper by Maxfield Research regarding this product. This highlights the many market responses to the alley loaded homes and why this product is no longer being proposed by developers in the Twin Cities. Mattamy, as a Canadian builder who builds in very dense urban and suburban locations, has rear loaded home product in our repertoire. Like other parts of the country, some urban planners in Canada believe it gives a much nicer streetscape than one dominated by "snout houses" (garage dominant). As a result, we are more or less forced to have this home product in our offerings.

Unfortunately, rather than this neighborhood design producing a pedestrian oriented space where neighbors interact, the result is a landscape entirely devoted to the auto in both the front and rear yard; the front yard is very small with a public street close to the home and a rear yard devoted entirely to the garage and alley. There is a very high impervious surface coverage ratio, the owner's personal space is encroached on, and development costs are very high due to all the streets, alleys, and other automobile accommodations. Maintenance costs and snow plowing difficulties also make these units a burden for the Homeowners Associations.

An additional aspect of the alley loaded product needs to be discussed with respect to Inspiration today. While studies vary, at the height of the "New Urbanist" craze, there was only 10-15% of the total U.S. market that was buying this style of home. It needs to be noted that there are still 12 vacant alley loaded lots left in Inspiration. To bring more alley loaded lots onto the market in this location would depreciate the values of existing homes of this type (supply vs. demand) and likely extend the period of time the existing lots remain vacant. Building more rear loaded product would not only be a bad market decision but would drive the value of the existing homes down; many of which have still not recovered their original value.

Reasons for Approval

The current proposal for Inspiration is reflective of the original goals and tenets approved by the City in 2004-5 while making considerations for today's housing styles, the desires of the overall market and the values of homes directly adjacent to the proposed development. There are no increases in density proposed; landscaping, open spaces, street layout and traffic patterns are all substantially the same. Mattamy believes that the proposal refines and improves the integration of the proposed open spaces into the neighborhood and streetscape, will complement the existing development and complete the neighborhood in a manner respectful of the original concept.

While not a legal basis for approval or denial, our proposal is being brought to the City with the "blessing" of the neighborhood. We have received both ARC and Board approval for our home plans and landscaping plans; residents serve on both these groups. The neighborhood likes the plan and what it will bring to their community.

Mattamy believes that the respect our plan shows of the original plans for Inspiration, and the acceptance of the current residents and their community merit approval of the proposal as presented.

Respectfully Submitted,

Rick Packer
Land Development Manager



March 10, 2014

Mr. Terry Forbord
Inspiration Land Holdings LLC
4960 Sussex Place
Shorewood, MN

RE: Alley-Loaded Lots at Inspiration in Bayport, Minnesota

Alley-loaded lots became popular in the late 1990s and early 2000s, primarily in high-density communities in the Eastern United States where in-fill and redevelopment were occurring in areas that already had an established street grid pattern. As the in-fill and redevelopment occurred however, buyers' desires for an attached garage changed the traditional alley-loaded lot design into something new, basically taking away the yard and attaching the rear-facing garage to the backside of the home.

Some more fully-developed eastern communities mandated that new in-fill developments have alley-loaded lots citing increased safety, better pedestrian access and increased aesthetics. However, builders are taking issue with many of these benefits, saying that the buying public does not perceive these enhancements with alley-loaded lots. In addition, builders state that putting in alleys reduces the number of total lots in the development and reduces the price points and total revenue to be generated from the subdivision because buyers perceive that alley-loaded homes should be less expensive than other home types.

Comments have also been made by national design review consultants that you typically do not see alley-loaded lots in northern climates because alleys are more difficult and more expensive to maintain for snow removal purposes.

Most of the alley-loaded lots that were developed in the Twin Cities were the focus of one developer that developed these types of lots in several subdivisions throughout the Metro. The vision for the alley-loaded lots was that they would promote increased neighborhood interaction and an increased pedestrian environment for residents. Buyers however, were not convinced and continued to exhibit a strong preference for the traditional front-loaded lot.

Although lot sizes are decreasing overall, buyers still expect to see some type of usable yard space in the back of their homes. Placing an attached garage at the back of the house reduces or eliminates the amount of yard space that is available and the perceived value of the property to the potential buyer.

Buyers are highly conscious of potential resale value when they purchase a home. Many buyers are unwilling to purchase a home or lot design that is outside of the traditional form because they are concerned that their appreciation value may be negatively affected by a design that would only attract a limited pool of future buyers.

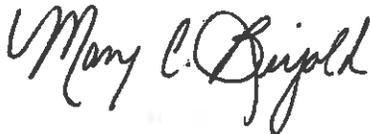
Most of the alley-loaded lots that were developed in these newer Metro Area subdivisions are maintained through association dues. Therefore, the upkeep and maintenance of the alleys are charged back to the homeowners through dues. The more alleys in the development, the higher the association dues needed to pay for their upkeep. In addition, homeowners that do not have alley lots in the same subdivision are likely to be reluctant to have their dues increased to pay for their upkeep and maintenance.

We strongly recommend that the Council consider changing the original plan for the Inspiration master-planned community to remove alley-loaded lots from the plan. Removing alley-loaded lots will enable the redesign of a lot and home product that will be highly attractive to future buyers and would result in increased absorption of lots in the Inspiration community.

If you have questions or need additional information, please contact me at (612) 904-7977.

Sincerely,

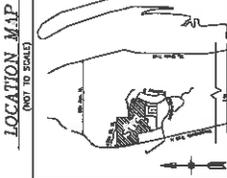
MAXFIELD RESEARCH INC.



Mary C. Bujold
President

INSPIRATION 3RD ADDITION

BAYPORT, MINNESOTA



COVER SHEET

INSPIRATION 3RD ADDITION

Bayport, Minnesota

MATTAMY HOMES, LLC

7201 Washington Avenue - Suite 201
Edina, MN 55439

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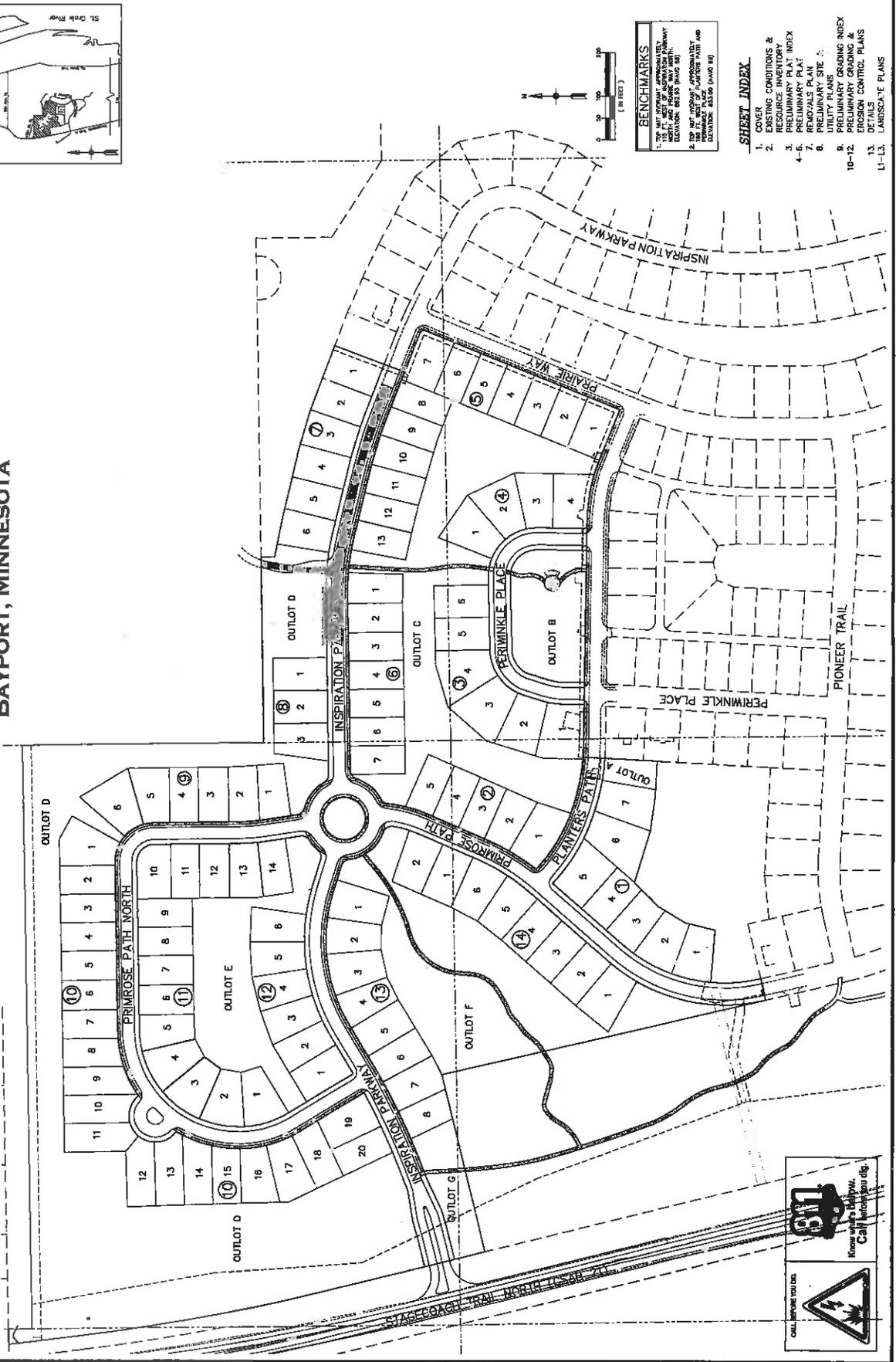
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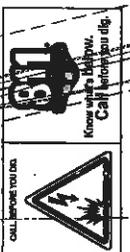
BENCHMARKS

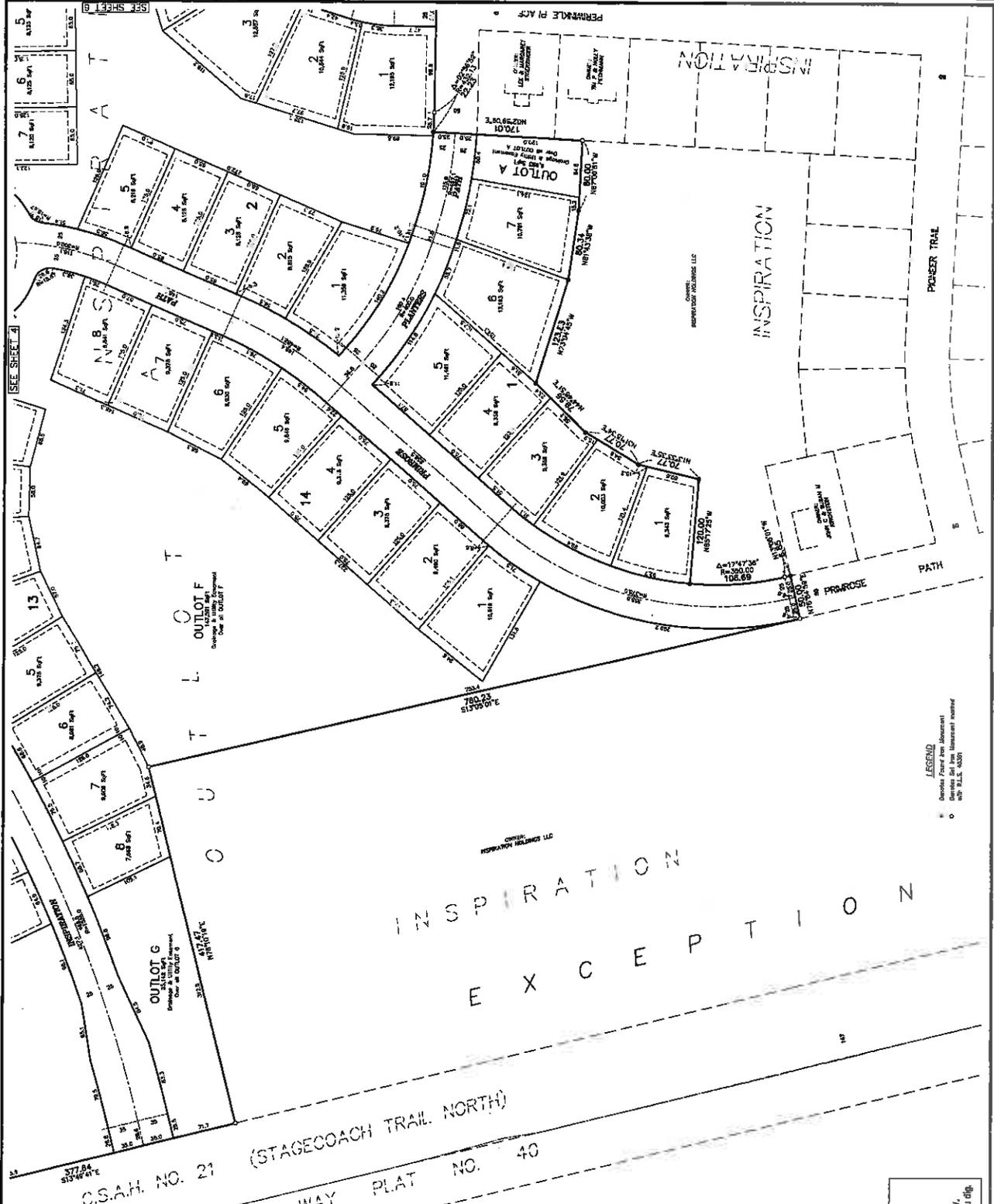
1. TO NOT BE REPAIRED APPROXIMATELY 10' NORTH OF PERIMINKLE PLACE ELEVATION: 855.5 (MAD 03)

2. TO NOT BE REPAIRED APPROXIMATELY 10' NORTH OF PERIMINKLE PLACE ELEVATION: 855.5 (MAD 03)

3. TO NOT BE REPAIRED APPROXIMATELY 10' NORTH OF PERIMINKLE PLACE ELEVATION: 855.5 (MAD 03)

- SHEET INDEX**
- COVER
 - EXISTING CONDITIONS & RESOURCE INVENTORY
 - PRELIMINARY PLAY INDEX
 5. PRELIMINARY PLAY
 6. PRELIMINARY SITE
 7. UTILITY PLANS
 8. PRELIMINARY GRADING INDEX
 9. PRELIMINARY GRADING & EROSION CONTROL PLANS
 - 10-12. PRELIMINARY GRADING & EROSION CONTROL PLANS
 13. LANDSCAPE PLANS
 - 14-15. LANDSCAPE PLANS





LEGEND

 * Denotes Easement

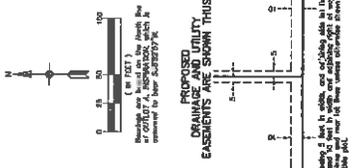
 o Denotes 60' line measurement marked with B.L.S. 4000

INSPIRATION

 EXCEPTION

C.S.A.H. NO. 21 (STAGECOACH TRAIL NORTH)

 HIGHWAY RIGHT OF WAY PLAT NO. 40



BENCHMARKS

 THE 10' WEST END OF INSPIRATION DRIVE

 IS THE BENCHMARK POINT

 150 FT. WEST OF PLANNERS PATH AND

 ELEVATION 565.00 (GAS 88)



 Know what's below.

 Call before you dig.



 CALL BEFORE YOU DIG



LEGEND

- Easement Easement
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BENCHMARKS

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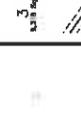
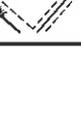
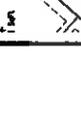
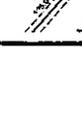
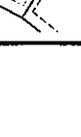
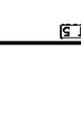
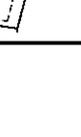
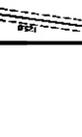
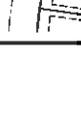
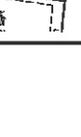
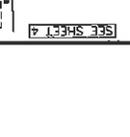
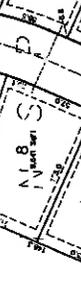
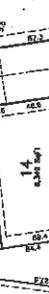
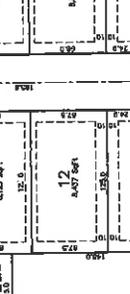
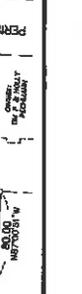
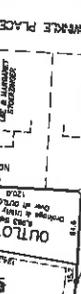
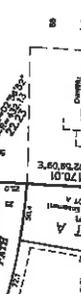
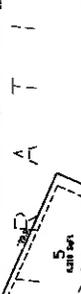
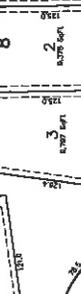
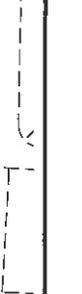
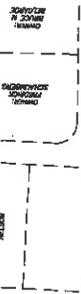
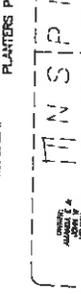
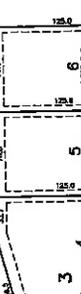
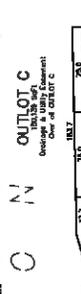
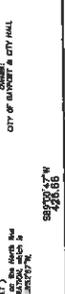
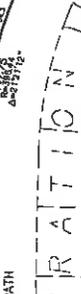
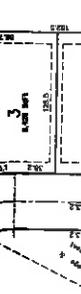
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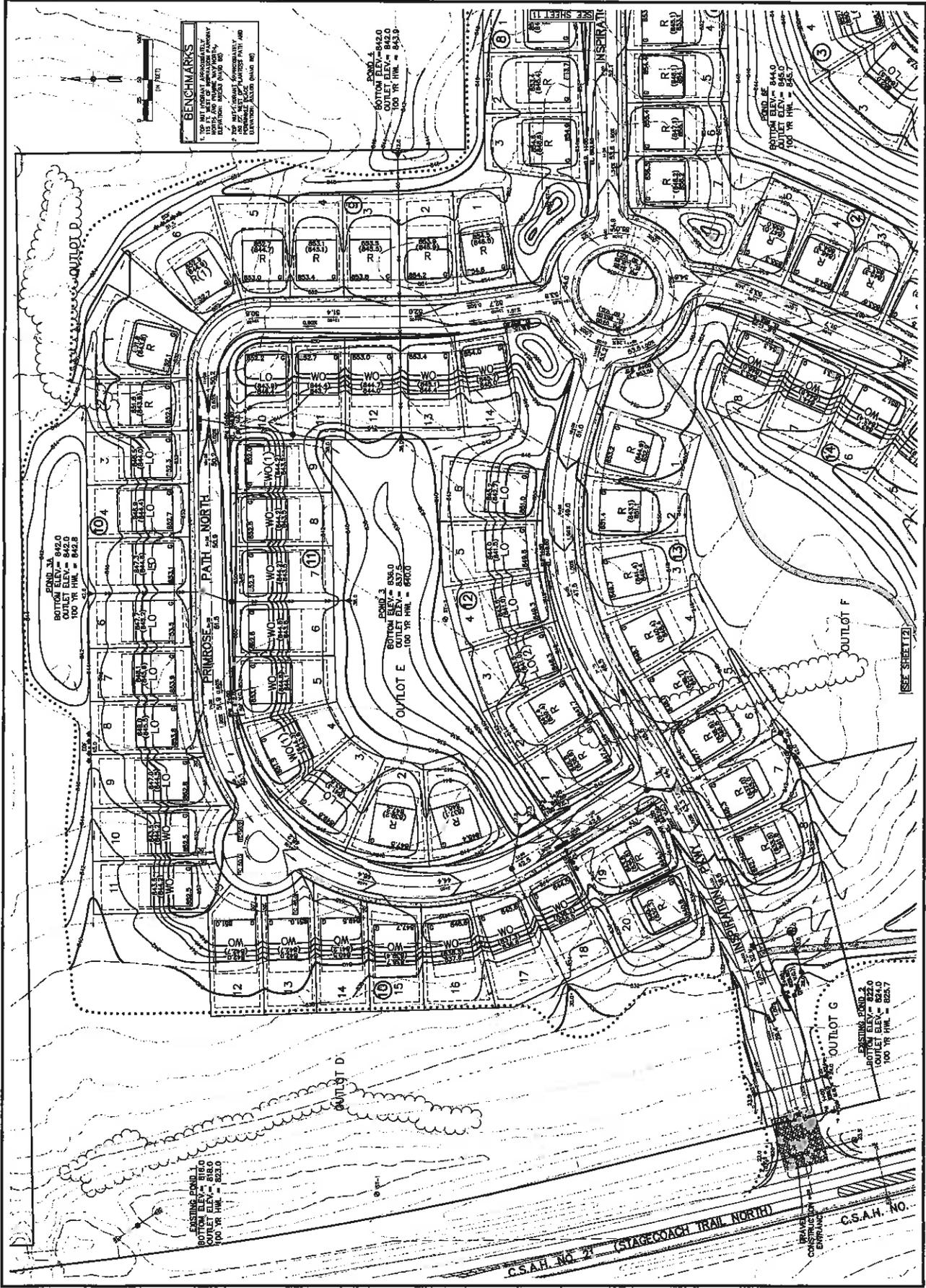
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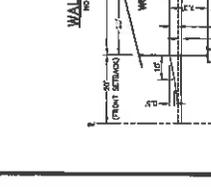
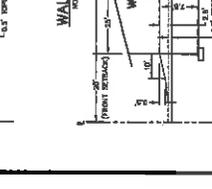
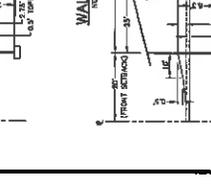
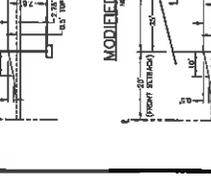
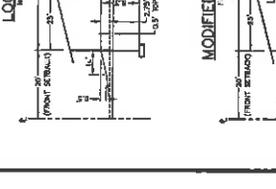
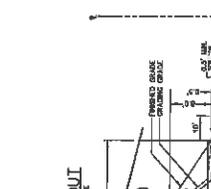
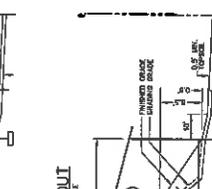
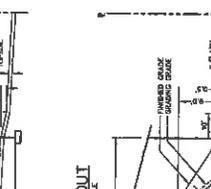
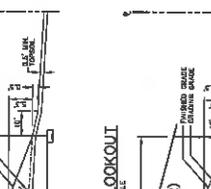
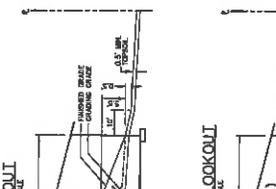
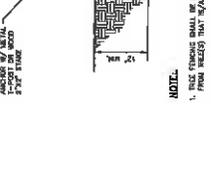
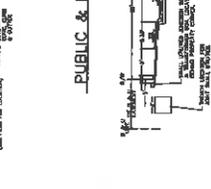
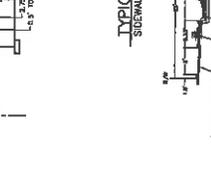
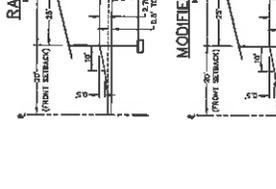
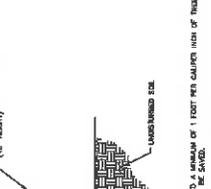
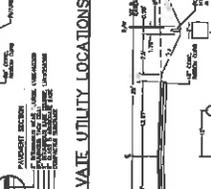
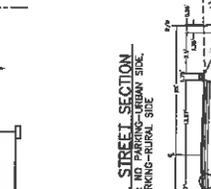
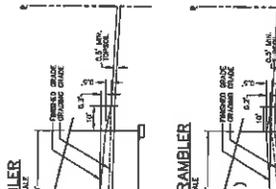
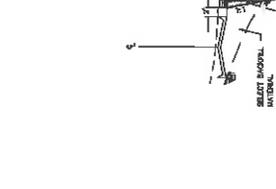
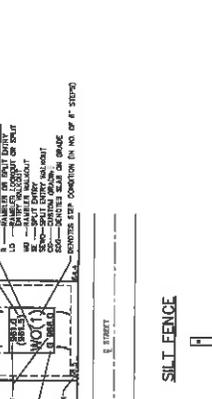
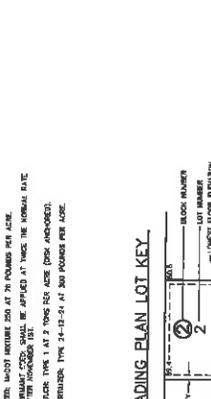
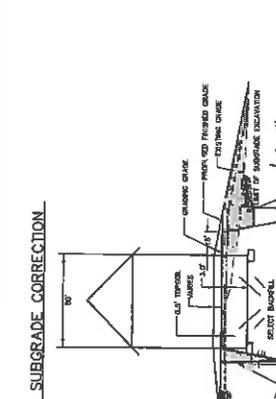
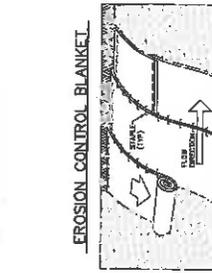
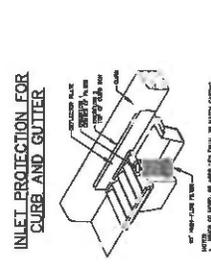
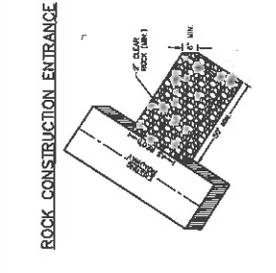
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NOTES:

1. PREPARE SOIL BEFORE INSTALLING RAMBLER. INCLUDE 1" APPLICATION
2. FILL IN THE TOP OF THE GORGE (OR CHANNEL) BY ALLOWING THE
3. BACK THE RAMBLER DOWN (STAYING PROPERLY) PROCEEDS
4. THE EDGE OF RAMBLER BLANKET MUST BE STAPLED WITH A
5. RAMBLER BLANKETS MUST BE STAPLED OVER THE GORGE. PLACE
6. RAMBLER BLANKETS OVER THE GORGE WITH WITHIN A
7. THE TRENCH LINE OF THE RAMBLER MUST BE PROTECTED BY A

NOTES:

1. USE A 2" X 4" BRANCH ALONG THE TRENCH. SET 1" FROM
2. SHOW ALL JOINDER JOINTS INTO THE GORGE AT THE DOWNHILL SIDE OF THE TRENCH.
3. JOINTS SHALL BE PLACED A MAXIMUM OF 6 FEET APART.
4. LAY OUT SET TRENCH ALONG THE UPHILL SIDE OF THE JOINDER JOINTS AND STAPLE 2" X 4" TRENCH
5. TRENCH JOINTS SHALL BE PROTECTED BY A MINIMUM OF THREE ATTACHMENTS PER JOINT.
6. SEE SHEET SPECIFICATIONS 2572 & 2586.

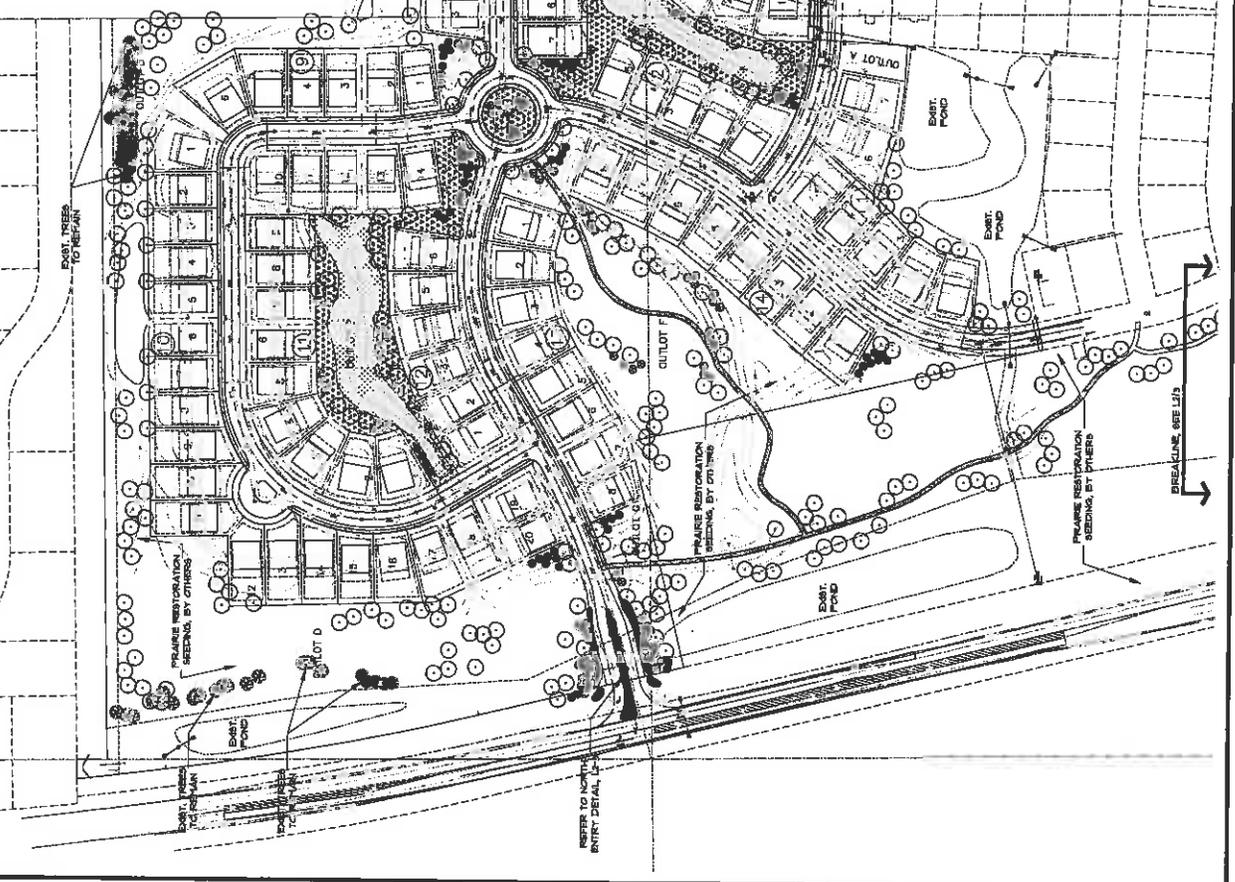
NOTE:

1. TREE TRENCH SHALL BE PLACED A MAXIMUM OF 1 FOOT FROM GORGE LINE IF TREE NUMBER
2. FROM TRENCH MAY BE SPACED UP TO 10 FEET APART.
3. JOINTS SHALL BE PLACED A MAXIMUM OF 6 FEET APART.
4. LAY OUT SET TRENCH ALONG THE UPHILL SIDE OF THE JOINDER JOINTS AND STAPLE 2" X 4" TRENCH
5. TRENCH JOINTS SHALL BE PROTECTED BY A MINIMUM OF THREE ATTACHMENTS PER JOINT.
6. SEE SHEET SPECIFICATIONS 2572 & 2586.

PRELIMINARY PLANT SCHEDULE

CODE	QTY	CORPORAL NAME / LANDSCAPE	SIZE	NOTES	ORIGINATOR
A	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
B	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
C	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
D	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
E	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
F	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
G	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
H	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
I	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
J	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
K	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
L	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
M	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
N	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
O	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
P	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
Q	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
R	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
S	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
T	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
U	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
V	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
W	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
X	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
Y	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1
Z	14	SHADY BARK OAK	2" CAL.	10 FT	STURMUTZ LEADER 1

- SYMBOL KEY**
- MAINT. NEED 1/4" DIA. 30-40'
 - MAINT. NEED 1/2" DIA. 30-40'
 - MAINT. NEED 3/4" DIA. 30-40'
 - MAINT. NEED 1" DIA. 30-40'
- PLANT KEY**
- DECIDUOUS TREE
 - ORNAMENTAL TREE
 - EVERGREEN TREE
 - VARIOUS SHRUBS
- NOTES:**
- PRAIRIE RESTORATION SEEDS BY OTHERS PER MANAGEMENT PLAN FOR OUTLOTS A, C, E & I.
 - SEED OUTLOTS B, F & G CRITERIA OF ROADABOUT WITH NATIVE PLANT SPECIES.
 - TOP SOIL PREPARATION AND SEEDING WILL BE PROVIDED BY BUILDER UPON COMPLETION OF HOME CONSTRUCTION.
 - SEE FINAL LANDSCAPE PLAN FOR PLANT SELECTIONS AND QUANTITIES.



BENCHMARKS

- 1. 100 FT. WEST OF PROPERTY LINE
- 2. 100 FT. WEST OF PROPERTY LINE
- 3. 100 FT. WEST OF PROPERTY LINE
- 4. 100 FT. WEST OF PROPERTY LINE
- 5. 100 FT. WEST OF PROPERTY LINE
- 6. 100 FT. WEST OF PROPERTY LINE
- 7. 100 FT. WEST OF PROPERTY LINE
- 8. 100 FT. WEST OF PROPERTY LINE
- 9. 100 FT. WEST OF PROPERTY LINE
- 10. 100 FT. WEST OF PROPERTY LINE

CALL BEFORE YOU DIG

811

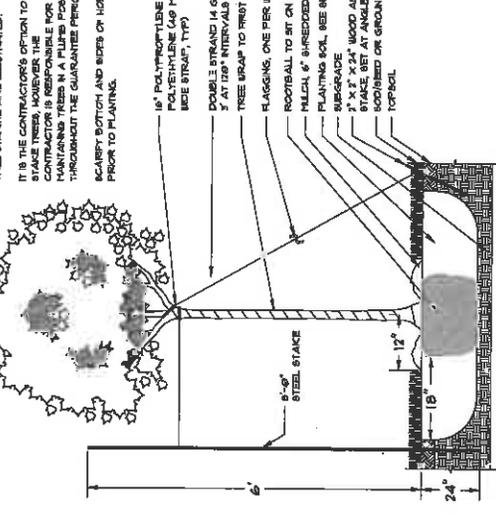
Know what's below. Call before you dig.

GENERAL NOTES:

- LANDSCAPE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS ON THE PROPERTY WITH THE UTILITY CONTRACTOR AND BY Gopher State One Call PRIOR TO STAKING PLANT LOCATIONS.
- COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON THE SITE.
- WHERE EXISTING TREES AND/OR SIGNIFICANT SHRUBS YASAGAS ARE FOUND ON THE SITE WHETHER SHOWN ON THE DRAWINGS OR NOT, THEY SHALL BE PROTECTED AND MAINTAINED UNLESS NOTED TO BE REMOVED AND/OR ARE WITHIN THE GRADING LIMITS. ANY QUESTION REGARDING THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO REMOVAL.
- ALL EXISTING TREES TO REMAIN SHALL BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD AND DAMAGED OR RUBBING BRANCHES.
- IF TREES AND SHRUBS ARE BALLED AND BURLAPPED.
- NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS OBTAINED BY THE LANDSCAPE ARCHITECT BY THE LANDSCAPE CONTRACTOR PRIOR TO SUBMISSION OF BID AND/OR GRAFTING.
- ALL PLANT MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AMERICAN ASSOCIATION OF NURSERYMEN.
- THE CONTRACTOR IS RESPONSIBLE FOR ON-GOING MAINTENANCE OF ALL NEWLY INSTALLED MATERIALS UNTIL THE DATE OF OWNER ACCEPTANCE. ANY ACTS OF VANDALISM OR DAMAGE TO OCCUR PRIOR TO OWNER ACCEPTANCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR SHALL PROVIDE A WRITTEN REQUEST FOR THE OWNER ACCEPTANCE INSPECTION.
- WARRANTY FOR THE LANDSCAPE MATERIALS SHALL BEGIN ON THE DATE OF ACCEPTANCE BY THE LANDSCAPE ARCHITECT AFTER THE COMPLETION OF PLANTING ALL LANDSCAPE MATERIALS. NO PARTIAL ACCEPTANCE WILL BE CONSIDERED.

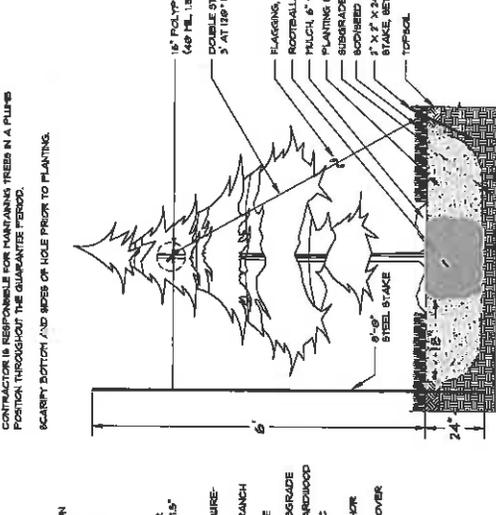
CONTRACTOR SHALL GUARANTEE NEW PLANT MATERIAL THROUGH ONE CALENDAR YEAR FROM THE DATE OF OWNER ACCEPTANCE WITH ALL REPLACEMENTS TO BE PROVIDED AT NO ADDITIONAL COST TO THE OWNER.

DECIDUOUS TREE



1 level site

CONFEROUS TREE



2 level site

PLANTING SPECIFICATIONS:

- PLANTING BED PREPARATION: ALL MASS PLANTING BEDS SHALL BE TILLED TO A MINIMUM DEPTH OF 18". AMENDMENTS SHALL BE APPLIED AFTER CULTIVATION.
- BACKFILL: USE SOIL EXCAVATED FROM PLANTING HOLES & PROVIDE AMENDMENTS, REMOVE ALL DEBRIS AND ROCKS LARGER THAN 3" DIA.
- REMOVAL: ALL PLANT MATERIALS SHALL BE FERTILIZED UPON INSTALLATION WITH 20-20-20 FERTILIZER AT 10 LBS PER 1000 SQ FT. PERMANENT PLANTING BEDS SHALL BE FERTILIZED WITH 20-20-20 FERTILIZER MIXED IN WITH PLANTING SOIL PER THE MANUFACTURER'S INSTRUCTIONS.
- MULCH MATERIAL: ALL MULCH MATERIALS SHALL BE FERTILIZED UPON INSTALLATION WITH 20-20-20 FERTILIZER AT 10 LBS PER 1000 SQ FT. PERMANENT PLANTING BEDS SHALL BE FERTILIZED WITH 20-20-20 FERTILIZER MIXED IN WITH PLANTING SOIL PER THE MANUFACTURER'S INSTRUCTIONS.
- TREE STAKING: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO STAKE AND/OR GUY THE TREES ACCORDING TO THE DETAILS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE EVERY STEP NECESSARY TO MAINTAIN THE TREES AND SHRUBS ON AN UPRIGHT AND PLUMB CONDITION AT ALL TIMES UNTIL THE END OF THE PLANT GUARANTEE PERIOD, ESPECIALLY WHERE VANDALISM, SOIL OR BIRD CONDITIONS ARE A PROBLEM.
- TREE WRAPPING: UNWARRANTED MATERIAL SHALL BE QUALITY, HEAVY WATERPROOF CREPE PAPER MANUFACTURED FOR THIS PURPOSE. WRAP ALL DECIDUOUS TREES IN THE FALL PRIOR TO 1/1 AND REMOVE ALL WRAPPING BY 1/1.
- RODENT PROTECTION: PROVIDE ON ALL TREES, EXCEPT SPRUCE UNLESS OTHERWISE NOTED.
- PLANTING PLAN: ALL PROPOSED PLANTS SHALL BE LOCATED CAREFULLY AS SHOWN ON THE PLANS. THE PLAN TAKES PRECEDENCE OVER THE NOTES. RESPECT NOTED DIMENSIONS. DO NOT SCALE DRAWINGS.
- EDGING: EDGING SHALL BE 6" PVC CONCRETE AS CASTE BEGINS WITH THREE (3) METAL ANCHOR STAKES PER 24 FOOT SECTION. UNLESS OTHERWISE NOTED, ALL MASS PLANTING BEDS SHALL HAVE EDGING PLACED BETWEEN MULCH AND ADJACENT TURF AREAS.

NOTE TWO (2) ALTERNATE METHODS OF TREE STAKING ARE ILLUSTRATED. IT IS THE CONTRACTOR'S OPTION TO STAKE TREES, HOWEVER THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING TREES IN A PLUMB POSITION THROUGHOUT THE GUARANTEE PERIOD.

SCARIFY BOTTOM AND SIDES OF HOLE PRIOR TO PLANTING.

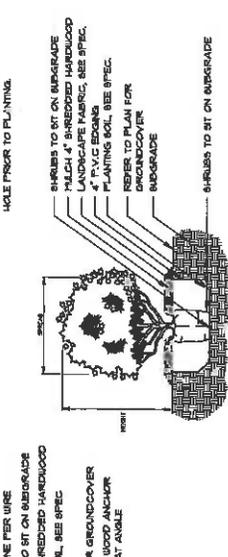
PLANTING NOTES:

- NO PLANTING SHALL BE INSTALLED UNTIL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE PLANTING AREA.
- ALL PLANT MATERIAL LOCATIONS MUST BE REVIEWED AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO ANY AND ALL DRAINAGE.
- IF THE LANDSCAPE CONTRACTOR PERCEIVES AND DEFICIENCIES IN THE PLANT SELECTIONS, SOIL CONDITIONS OR OTHER SITE CONDITIONS WHICH MIGHT NEGATIVELY AFFECT PLANT MATERIAL SURVIVAL, THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- ADJUSTMENTS IN THE LOCATION OF PROPOSED PLANT MATERIAL MAY BE NEEDED IN THE FIELD. SHOULD AN ADJUSTMENT BE ADVISED, THE LANDSCAPE ARCHITECT MUST BE NOTIFIED.
- ALL PLANTS TO BE INSTALLED PER PLANTING DETAILS.
- ONE SHURB PER TYPE AND SIZE IN EACH PLANTING BED AND EVERY TREE SHOULD BE CLEARLY IDENTIFIED COMMON OR TRADE NAMES WITH A PLASTIC TAG WHICH SHALL NOT BE REMOVED PRIOR TO OWNER ACCEPTANCE.
- WHERE SOIL-BASED ARBORS PLANTED SURFACE, FINISHED GRADE OF BEDDING SHALL BE HELD 1" BELOW SURFACE ELEVATION OF FINAL, SLAB, CURBS, ETC.
- SOIL SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. ON SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES, THE SOIL SHALL BE STAKED TO THE GRADE.
- SEED ALL AREAS DISTURBED DUE TO GRADINGS OTHER THAN THOSE TO RECEIVE SOIL.
- REPAIR ALL DAMAGE TO PROPERTY FROM PLANTING OPERATIONS AT NO COST TO THE OWNER.

IRRIGATION NOTES:

- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SLOTTED DRAINAGE ILLUSTRATIONS IRRIGATION PLANS AND SPECIFICATIONS AS PART OF THE SCOPE OF WORK WHEN BIDDING. THESE SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO ORDER.
- VERIFY EXISTING/PROPOSED SYSTEM LOCATION.
- UNLESS OTHERWISE NOTED, ALL SOIL/SEED AND PLANTING AREAS INDICATED ON THE PLAN SHALL BE IRRIGATED. THE CONTRACTOR SHALL INCLUDE THE PUBLIC RIGHT OF WAY BETWEEN THE PROPERTY LINE AND BACK OF CURB WHERE NEW LANDSCAPE IMPROVEMENTS ARE PROPOSED.
- IT SHALL BE THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY TO INSURE THAT ALL SOIL/SEED/BEED AND PLANTING AREAS ARE IRRIGATED PROPERLY, INCLUDING THOSE AREAS DIRECTLY AROUND AND ADJACENT BUILDING FOUNDATION.
- THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER WITH A WATERING/LAWN IRRIGATION SCHEDULE APPROPRIATE TO THE PROJECT SITE CONDITIONS AND TO PLANT MATERIAL GROWTH REQUIREMENTS.

NOTE: HAND LOOSEN ROOTS OF CONTAMINATED MATERIAL (TOP). SCARIFY BOTTOM AND SIDES OF HOLE PRIOR TO PLANTING.

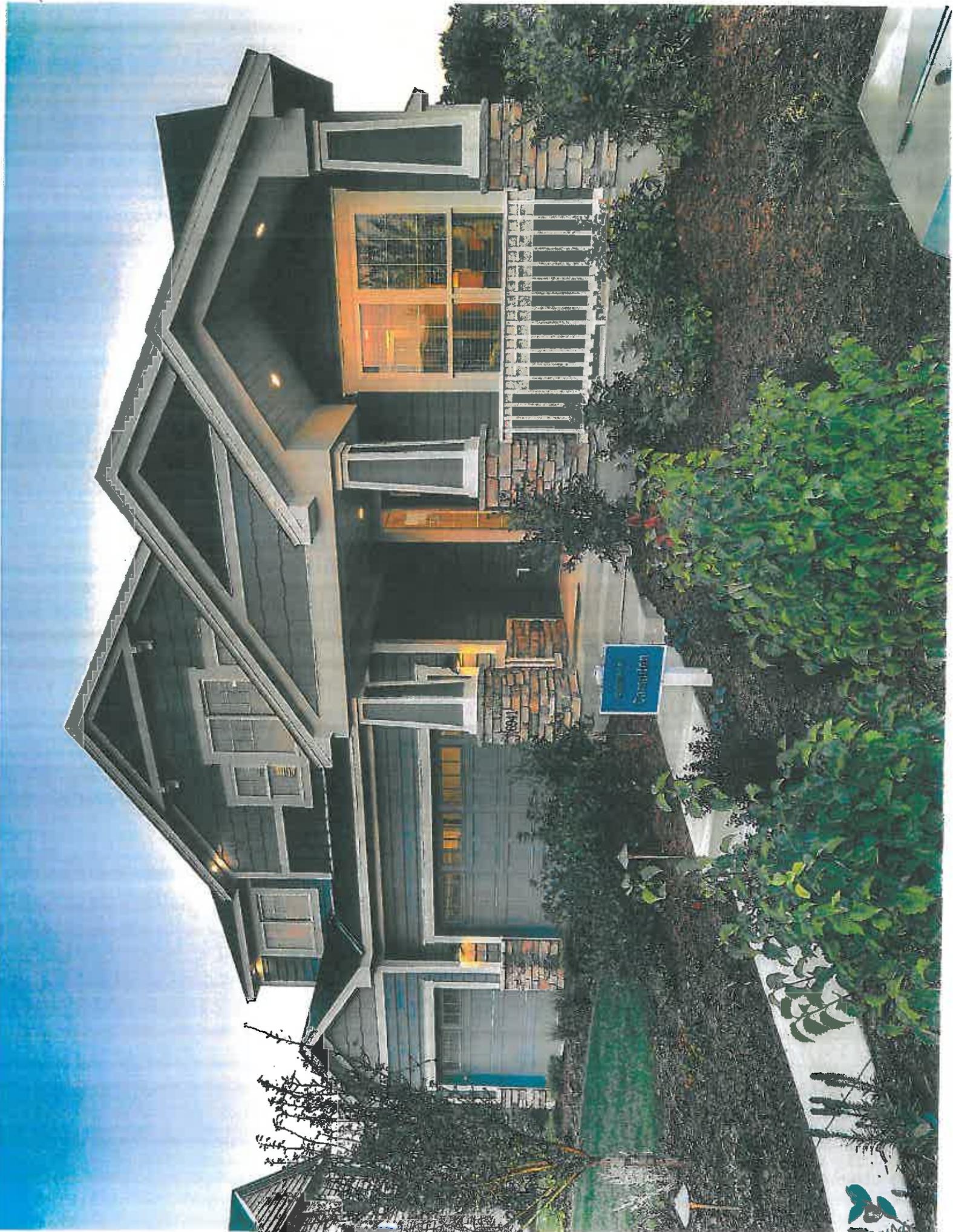


3 BURBUB



CONCEPT PLAN BY MATTAMY HOMES



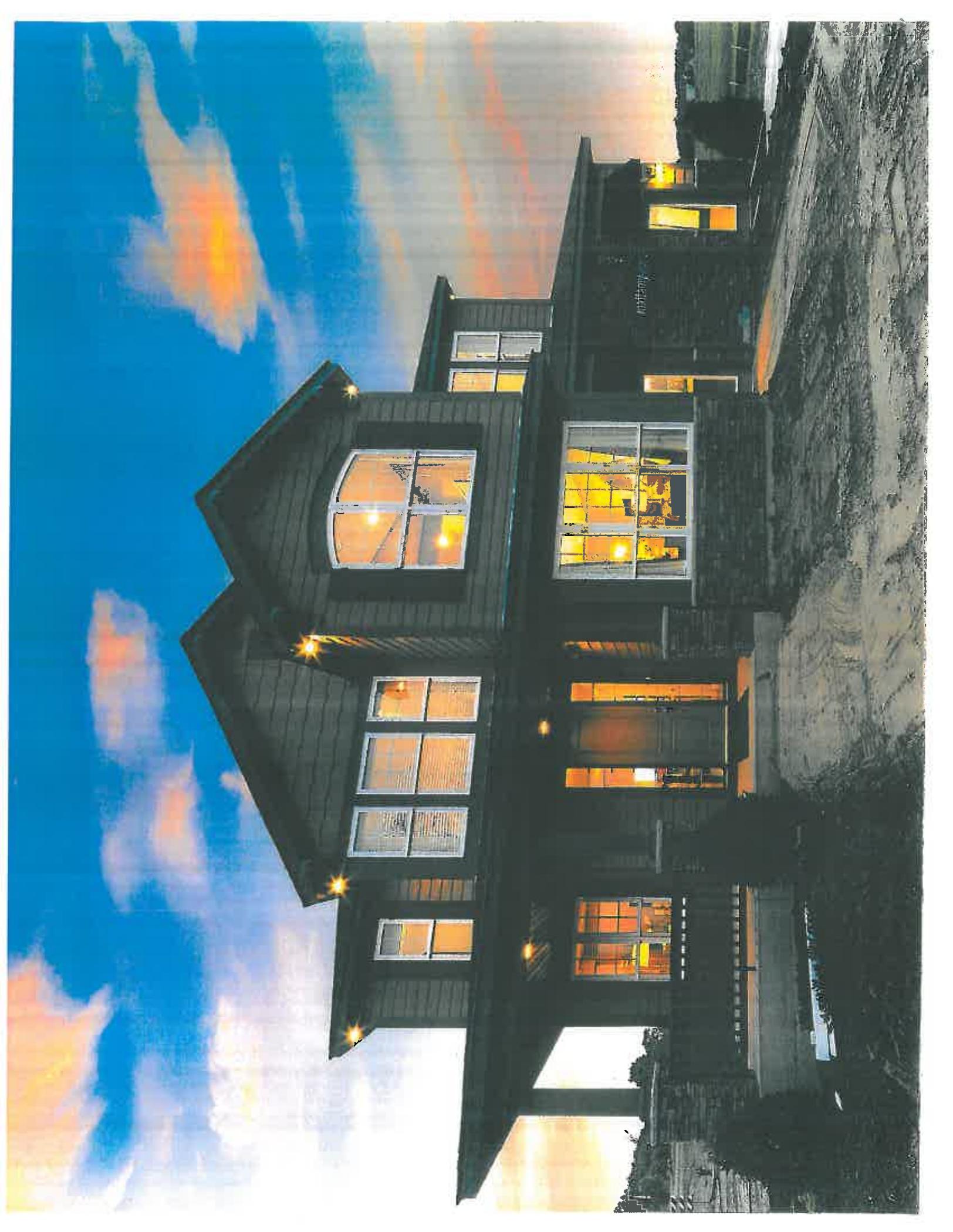














Building a Better World
for All of Us®

MEMORANDUM

TO: Sara Taylor | Assistant Administrator/Planner
Daniel Licht, The Planning Company

FROM: John D. Parotti, PE | City Engineer

DATE: March 10, 2015

RE: Inspiration Phase III B - Preliminary Plat Review
SEH No. BAYPO 131711 14.00

The Preliminary Plat for Inspiration - 3rd Addition as prepared by Carlson McCain, dated February 13, 2015, has been reviewed for engineering issues. In general, the submittal is complete. The following comments are provided for consideration:

GENERAL COMMENTS

1. The project is required to comply with the overall stormwater management concepts in the previously approved Inspiration documents which require conservation techniques be used. The stormwater management concepts proposes to use naturally vegetated swales and stormwater management/collection areas along with storm sewer and larger infiltration basins to meet this intent.
2. The project qualifies for review by the Middle St. Croix Watershed Management Organization (WMO) review and performance standards. Compliance with WMO performance standards should be made a condition of City approval.
3. State permits will be required for the sanitary sewer and water main extensions. An Access Permit and Utility Permit will be required from Washington County for the Inspiration Parkway North connection to Stagecoach Trail and for the water main improvements within Stagecoach Trail. It is the Developer's responsibility to apply for and obtain the permits and to construct improvements as required by the State and County.
4. The proposed development will generate about 1,080 trips per day. There will be about 85 trips in the AM peak and 95 in the PM peak. About 60% of the trips are expected to be to and from the north and 40% to the south on CSAH 21.

In the AM peak, about 45 vehicles will use Inspiration Parkway North and 40 vehicles will use Primrose Path South and Inspiration Parkway South. In the PM peak, about 72 vehicles will use Inspiration Parkway North and 23 will use Primrose Path South and Inspiration Parkway South.

Because there is a traffic signal at Inspiration Parkway South at Stagecoach Trail, some of the residents will chose to travel to that intersection to turn south. It is estimated that only about 100 vehicles per day would turn left from Inspiration Parkway North onto Stagecoach Trail if the turn were allowed. The remaining 116 would travel on Primrose Path (about 13 would use Prairie Way) to reach Inspiration Parkway South and the traffic signal at Stagecoach Trail.

5. The roundabout/traffic circle design requires further discussion and design development. As submitted, there are no design elements that will discourage or prevent drivers from turning left.

Engineers | Architects | Planners | Scientists

Short Elliott Hendrickson Inc., 156 High Street, Suite 300, New Richmond, WI 54017-1128

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Elements such as splitter islands and a narrower drive lane should be considered for this purpose. Also, design vehicle information and turning movements shall be submitted for review. School busses and garbage trucks should be able to navigate the roundabout on the drive lane. Moving vans and other vehicles with trailers would be allowed to use the center island apron for trailer wheels. Analysis of how Bayport Fire Department vehicles can navigate the roundabout shall be submitted. The roundabout design should be modified and resubmitted with the vehicle turning analysis for further review and comment.

6. The plan calls for a partial cul-de-sac (eyebrow) at Lots 10 - 13, Block 10. Eyebrows present snow removal and other traffic and maintenance challenges. It's recommended that the eyebrow be removed and the Block 10 lots reconfigured.
7. Lighting, signing and striping plan should be submitted for review.
8. Signing and striping plan should include No Parking sign locations in addition to all other required traffic and street name signs.
9. Sanitary sewer flows will be conveyed to the lift station installed with previous phases of the Inspiration Development. Lift station design should be reviewed to confirm that capacity exists for this development as well as the potential for up to 75 condo/multifamily units as presented in the original PUD.

SHEET 7 - REMOVALS PLAN

10. Provide detail of how existing services will be abandoned. All abandoned pipes still connected to a main, shall be made field locatable by installing tracer wire or other acceptable means. At a minimum, curb stops shall be shut off and capped and sanitary sewer service lines shall be cut at the right-of-way line and capped with a cemented-on cap. Abandoned service locations shall be recorded using both ties and latitude/longitude or County survey coordinates.
11. There are two notes on the west side of Prairie Way which say "ABANDON EX. SERVICE". Modify the notes to clarify if the services to be abandoned are water, sewer or both.

SHEET 8 - SITE AND UTILITY PLAN

12. Several of the sanitary sewer manholes will need to be constructed as drop structures. Final determination of where drop structures are required will occur as part of the final design and review.
13. All sanitary sewer design shall comply with the Ten State Standards for Wastewater Facilities.
14. There are two segments of sanitary sewer that are constructed parallel to a deeper sanitary sewer line. This approach results in more sewer pipe and structures than necessary to serve the subdivision and therefore higher long-term maintenance costs to the City. The design should be modified to eliminate the shallow sewer line and install sewer service risers to the proposed lots.
15. Sanitary sewer and water main is shown crossing diagonally across the southeast corner of Outlot B. This will result in these utilities being located in an outlot not owned by the City and will require the use of long service lines to serve Lots 3 & 4 of Block 4. The design should be modified to connect to the existing sewer and water main in the intersection of Periwinkle Place and Planters Path.
16. A fire hydrant shall be placed at the dead end water main west of the intersection of Stagecoach Trail and Inspiration Parkway. Hydrant shall be placed on the right-of-way line.
17. Due to the high likelihood of homes constructed close to side lot lines and the increase risk of a structure fire causing damage to adjacent structures, the Fire Department has required that hydrants be spaced at a maximum of 350' apart.
18. Gate valves shall be provided at a maximum spacing of 800' or a minimum of one per block as required by the Ten State Standards. Further, gate valves are to be installed in locations that allows water system operators to minimize inconvenience and sanitary hazards during repairs. Valves shall be added or locations shall be revised as follows:

- a. Intersection of Primrose Path and Inspiration Parkway: Valves are needed on all four legs of the water main cross to allow for isolation of 1-block segments of the main.
 - b. A gate valve shall be added on Primrose Path North at Sta. 7+50 ±.
 - c. The valve located at Sta. 5+65± Inspiration Parkway shall be moved to Sta. 3+75±.
 - d. Periwinkle Place: Gate valves shall be installed at both connections to the existing water main.
19. All gate valves and curb stops shall have stem risers.

SHEETS 9 - 12 - GRADING AND EROSION CONTROL PLANS

20. The City's Ordinance (Ch. 48) requires the use of sump catch basins and forebays to remove sediment before discharge to ponding areas. Detail with respect to sump catch basins can be provided at the time of construction plan submittal. Forebays or other pretreatment measure are not proposed by the current design and should be added and resubmitted for review.
21. The WMO review will likely result in changes to the grading and erosion control design to meet WMO performance standards. When those changes are made, the plans and supporting drainage calculations shall be updated and resubmitted to both the City and WMO for review.

SHEET 13 - DETAILS

22. Change the ribbon curb width to 20" in the utility locations detail.
23. Additional details will be required for construction document review.



Public Works Department

Donald J. Theisen, P.E.
Director

Wayne H. Sandberg, P.E.
Deputy Director/County Engineer

March 11, 2015

Sara Taylor
City Planner
City of Bayport
294 North Third Street
Bayport, MN 55003

RE: Washington County comments on the Preliminary Plat of Phase III-B Inspiration, City of Bayport

Dear Ms. Taylor

Washington County has reviewed the Preliminary Plat of Phase III-B Inspiration, City of Bayport dated 2/13/2015. The project will consist of 113 single family residential lots and 7 outlots. Since this parcel is along Washington County State Aid Highway (CSAH 21) Stagecoach Trail, the county has prepared the following comments from the Department of Public Works and permits required for improvements to CSAH 21.

The proposed project will generate an additional 1090 trips per day, *ITE Trip Generation, 7th Addition*, on CSAH 21 which is functionally classified as an "A" Minor Arterial Roadway. There is currently 5900 Average Daily Trips (ADT) on CSAH 21. Based on current traffic volumes, and existing infrastructure including the traffic signal at Inspiration Parkway and the St. Croix Preparatory Academy, the following improvements are necessary:

- Construction of a ¾ access at the new intersection of Inspiration Parkway and CSAH 21. These improvements include a NB right- turn lane, a SB center left-turn lane and associated pavement markings. The current shoulder widths need to be perpetuated. Since the existing shoulders do not have enough pavement depth to provide an adequate base for vehicular traffic, there will be removals, new sub base material and new pavement required.
- A raised median (pork chop island) shall be installed at the new Inspiration Parkway road section at CSAH 21 to channelize vehicular movements and prohibit left hand turns from Inspiration Parkway onto CSAH 21. The design shall meet the requirements of the MnDOT Road Design Manual.
- Access control should be dedicated to Washington County along the CSAH 21 frontage, except for the opening corresponding to the City's right-of-way for Inspiration Parkway.
- The developer or the city must submit the drainage report and calculations to our office for review of any downstream impacts to the county drainage system. Along with the

drainage calculations, we will request written conclusions that the volume and rate of stormwater run-off into the county right-of way will not increase as part of the project.

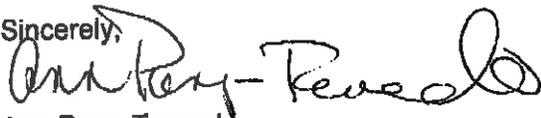
- There should be consideration for extending the trail system and providing a local street right-of-way through Outlot D for a future street connection to the CSAH 21/47th Street intersection via future residential development north of this property.
- Washington County's policy is to assist local governments in promoting compatibility between land use and highways. Residential uses located adjacent to highways often result in complaints about traffic noise. Traffic noise from this highway could exceed noise standards established by the Minnesota Pollution Control Agency (MPCA), the U.S. Department of Housing and Urban Development, and the U.S. Department of Transportation. Minnesota Rule 7030.0030 states that municipalities are responsible for taking all reasonable measures to prevent land use activities listed in the MPCA's Noise Area Classification (NAC) where the establishment of the land use would result in violations of established noise standards. Minnesota Statute 116.07, Subpart 2a exempts County Roads and County State Aid Highways from noise thresholds.

County policy regarding development adjacent to existing highways prohibits the expenditure of highway funds for noise mitigation measures in such areas. The developer should assess the noise situation and take any action outside of County right of way deemed necessary to minimize the impact of any highway noise.

- A Right-of Way permit and detailed plans for CSAH 21 will be required for the work in the county right-of way including grading for the installation of culverts, installation of water and sewer services if necessary, any paving and pavement markings for the left and right turn lanes on CSAH 21, any landscaping and other improvements on the county right-of-way.
- An access permit and plans for Inspiration Parkway will be required from the City of Bayport for access to CSAH 21/Stagecoach Avenue.
- The county strongly encourages a review of the proposed traffic circle design by the City Engineer and Fire Officials to make sure it can provide adequate turning movements for school buses and emergency vehicles. A roundabout or mini-roundabout may be a better option to accommodate such vehicles while still providing speed control.

If you have any questions or comments to the responses on the Inspiration Development, please contact me at Ann.Pung-Terwedo@co.washington.mn.us.

Sincerely,



Ann Pung-Terwedo
Senior Planner

Cc: Joe Gustafson, Traffic Engineer
Wayne Sandberg, County Engineer

MEMORANDUM

DATE: March 10, 2015

TO: Planning Commission (March 16, 2015 meeting)

FROM: Sara Taylor, Assistant City Administrator/City Planner

SUBJECT: Public hearing to consider a variance to allow an attached garage and addition to the existing single-family home on the property located at 207 6th Street North

A. BACKGROUND

The property is located at 207 6th Street North and is legally described as Lots 5 and 6, Block 53, Bayport, Washington County, Minnesota. The property dimensions are approximately 140 feet on the north and south lot lines, and 100 feet along the east and west lot lines, with a total lot area of 13,990 square feet. The subject property is surrounded by residential uses and is zoned R-2 Single Family Urban.

Property owner Kyle Carlson has submitted an application for a variance to allow a new attached garage and addition to the existing house on the property. The property contains an existing legally non-conforming two-story, single-family house and detached garage that do not meet current building setback requirements from the property lines. In addition, a portion of the existing detached garage encroaches into the city alley right-of-way. As proposed, the detached garage will be removed and replaced with a new, larger garage that will be attached to the house and in compliance with setback requirements. The applicant is also proposing to expand the footprint of the house to allow additional living space on the main floor. Because the existing house does not meet current setback requirements, a variance is required to allow expansion of a legally non-conforming structure to allow the proposed improvements.

Notice of the Planning Commission's public hearing was mailed to all property owners within 350 feet of the subject property and published in the Stillwater Gazette on March 4, 2015. The following informational items are attached:

- narrative by the property owner
- land survey of the property with existing conditions
- land survey of the property with proposed conditions

B. STAFF COMMENTS

As stated, the applicant is proposing to improve the existing single-family home with a main floor addition and a new attached garage. The existing house was built in 1930 and contains limited living space on the main floor. The proposed addition on the north side of the house would allow for a main floor master bedroom. The addition on the east side of the house would allow for a main floor bathroom, laundry area, and mudroom that would connect the house to a new attached 3+ stall garage. A portion of the existing driveway would remain in its current location and be supplemented with new concrete or asphalt to create a suitable parking/driveway surface to access the new attached garage.

This house is considered a legally non-conforming structure, because it was constructed prior to the adoption of the city's zoning code, and does not meet current zoning setbacks and/or standards. The zoning code allows legally non-conforming structures to continue to be used and be reasonably maintained. However, any alteration, expansion, or improvement to the structure beyond normal maintenance requires a variance.

A 20 foot front and side yard setback is required for a house on a corner lot abutting a city street right-of-way. The existing house is only set back approximately 12 feet on the front (west) property line and 14 feet on the side (south) property line. It should be noted that the proposed improvements will increase the footprint of the house, but will not increase its non-conformity with city code because the addition and attached garage will meet current setback, height, and impervious coverage requirements. In addition, the existing non-conforming detached garage will be removed, which will further bring the property into compliance with current zoning requirements, and is a significant improvement over the existing conditions.

	CITY CODE	EXISTING	PROPOSED	OUTCOME
Lot area	Min. 10,000 sq. ft.	13,990 sq. ft.	13,990sq. ft.	Complies with code
Impervious lot coverage	Max. 35%	22%	34.81% (includes removing existing detached garage and adding new main floor area/attached garage)	Complies with code
Accessory structures/garage	Max. 10% of lot area (1,399 sq. ft. for this lot)	760 sq. ft.	1,392 sq. ft.	Complies with code
House	Up to 4,897 sq. ft. (including impervious and garage)	880 sq. ft.	1,586 sq. ft.	Complies with code
Front (west) setback	20 ft.	12 ft.	12 ft. (for existing house) 22 ft. (for addition)	Complies with code
Side (south) setback	20 ft.	14 ft.	14 ft. (for existing house) 21 ft. (for addition)	Complies with code
Side (north) setback	10 ft.	58 ft.	38 ft. (for addition)	Complies with code
Rear (east) setback	30 ft.	98 ft.	38 ft. (for attached garage)	Complies with code

C. SUGGESTED FINDINGS OF FACT AND CONDITIONS OF APPROVAL

Minnesota Statutes 462.357. Subd 6.(2) states in part that "Variances shall only be permitted when they are in harmony with the general purposes and intent of the ordinance and when the variances are consistent with the comprehensive plan. Variances may be granted when the applicant establishes that there are practical difficulties in complying with the zoning ordinance. "Practical difficulties," as used in connection with the granting of a variance, means that the property owner proposes to use the property in a reasonable manner not permitted by the zoning ordinance; the plight of the landowner is due to circumstances unique to the property not created by the landowner; and the variance, if granted, will not alter the essential character of the locality. Economic considerations alone do not constitute practical difficulties." This language establishes the criteria

by which the Planning Commission is to consider the variance application. If the variance is determined to meet the criteria, a request for the variance should be granted.

The proposed addition to the existing house is reasonable, and will provide both additional living space on the main floor and more accessible garage and storage area. Because the existing house was built before the city adopted a zoning code, it does not comply with current setback requirements. However, it should be noted that these circumstances were not created by the landowner. The setbacks and height of the main floor addition will be similar to other residential structures in the neighborhood. Although the proposed garage will be attached to the house and is sizable, it will face the alley similar to other garages in the neighborhood and also be in compliance with setback, height, and impervious coverage requirements. For these reasons, staff finds that the criteria for the variance have been met and therefore is recommending approval of the variance, with the following conditions:

- ❑ The proposed improvements will be subject to review and recommendations by the Middle St. Croix Management Organization.
- ❑ Prior to the issuance of a building permit, a complete set of construction plans for the new main floor addition/attached garage shall be submitted by the applicant, in accordance with the approved variance, for review and approval by city staff.
- ❑ The existing detached garage shall be removed from the property prior to issuance of a certificate of occupancy and use of the new attached garage.
- ❑ All parking and driveway areas must be surfaced with asphalt, concrete, or pavers of an equivalent material. All non-parking areas shall consist of sod, landscaping, or equivalent pervious material.
- ❑ The type and color of the exterior finishes for the existing house and new addition/attached garage shall compliment each other.
- ❑ With the exception of the variance to allow the new addition/attached garage, any proposed or future improvements to the property must comply with all zoning and general city ordinances.
- ❑ An as-built survey of the property will be required to be submitted to the city to certify compliance with the approved variance prior to issuance of a certificate of occupancy and use of the new addition/attached garage.
- ❑ Due to a proposed impervious coverage of approximately 35%, which is the maximum coverage permitted by city code, no temporary storage structures, canopies, parking of passenger or recreation vehicles, or other conditions that would result in an increase of impervious coverage shall be allowed on the property. All vehicles and equipment shall be parked and/or stored within the attached garage or on the asphalt/concrete driveway surface.

D. *RECOMMENDATION*

Staff recommends approval of a variance to allow the existing legally non-conforming house on the property to be expanded with a main floor addition and attached garage, as proposed by the applicant. Suggested findings of fact and conditions of approval are stated in section "C" of the staff report. The Planning Commission is asked to make a recommendation on the application for City Council consideration. The City Council will consider the variance at its meeting on April 6, 2015.

February 4th, 2015

Narrative for a variance on the property at 207 Nth 6th Street

My name is Kyle Carlson and I reside at 207 Nth 6th Street Bayport, Minnesota. It is the above property that I am writing to request a variance for. The home was built in 1930 with an updated foundation in the 70's. The main floor living space is extremely small 26' x 26'. The garage is about 75' from the house and 3' to 4' over the property line. This makes it very inefficient when making trips to and from the house. I have lived in Bayport my whole life and have served on the Bayport Fire Department for the past 10 years. I enjoy living and volunteering in the community and plan to be here for a while. In order to do this I have to make some improvements to my property. The improvements will be an investment for my future, plus make the house and garage more efficient and user friendly.

My proposal, includes a main floor addition on the north side of the house. This north addition would be a master bedroom. The proposal also includes an addition to the east that would be a main floor bathroom, laundry room, mudroom entrance and an attached garage. These additions would give us more living space for future growth, more efficient use of the home and convenient indoor parking. My property is large enough to accommodate my proposal while still meeting all setbacks and impervious surface requirements of the city.

In conclusion I am applying for this variance so that I can improve my property to meet my future needs. It is important to me to be able to stay living and volunteering in the community of Bayport.

Thank You,

Kyle Carlson

CERTIFICATE OF SURVEY

Land Description
Warranty Deed, Doc. No. 3509928

Lots Five (5) and Six (6) in Block
Fifty-three (53) of Bayport,
Washington County, Minnesota.

Lot 5 and Lot 6, Block 53, BAYPORT
(Platted as SOUTH STILLWATER)
City of Bayport, Washington County, Minnesota

Boundary Survey
Existing Conditions
Prepared for:

Kyle R. Carlson
207 6th Street North
Bayport, Minnesota 55003

PID: 10-029-20-14-0008

Northeast corner
Section 10, T29N, R20W.

Boundary Legend

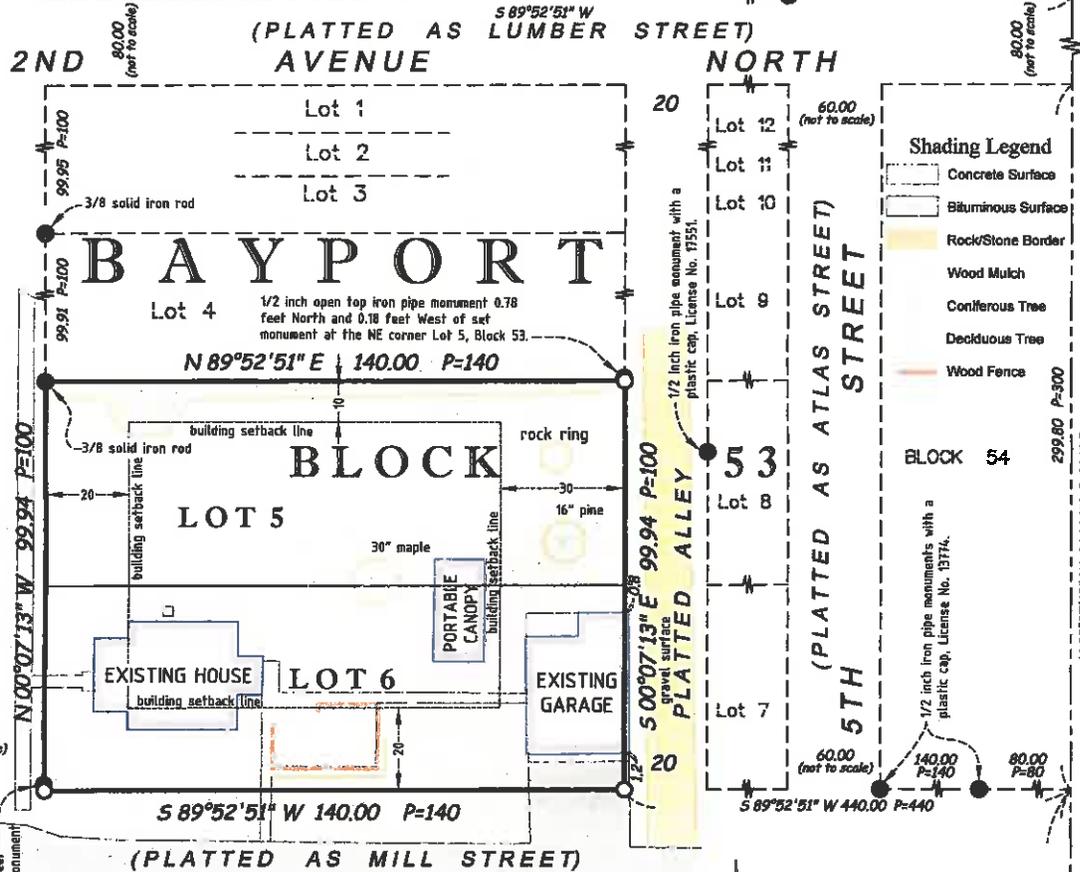
- Denotes set 1/2 inch iron pipe, 18 inches long, with a plastic cap inscribed ANEZ LS 13775.
 - ⊙ Denotes set 1/2 inch iron pipe, 18 inches long, with a plastic cap inscribed ANEZ RLS 13775, set on a survey dated December 08, 1989.
 - Denotes found iron monument as noted.
 - ⊕ Denotes cast iron Washington County Section Corner Monument.
- P=* Refers to recorded distances on the plat of BAYPORT (platted as SOUTH STILLWATER) on file and of record in the Office of the County Recorder, Washington County, Minnesota.
- Distances are in feet and decimals of a foot.
- Bearings are based on the Washington County Coordinate System, NAD 83, 1986 Adjustment.

UN-NUMBERED BLOCK
1/2 inch iron pipe monument with a plastic cap, License No. 13774.
60.00 (not to scale)

NORTH
BLOCK 47
60.00 (not to scale)

BLOCK 52
(PLATTED AS TURTLE STREET)
NORTH
STREET

BLOCK 54
(PLATTED AS ATLAS STREET)
NORTH
5TH STREET

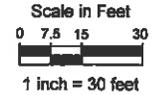


Shading Legend

- Concrete Surface
- Bituminous Surface
- Rock/Stone Border
- Wood Mulch
- Coniferous Tree
- Deciduous Tree
- Wood Fence

Parcel Area: 13,990 sq. ft.

Existing Coverage	Area	% of Parcel Area
House and Garage	1,640 sq ft	11.7%
Concrete/Bituminous	961 sq ft	6.9%
Portable Canopy	303 sq ft	2.1%
Landscape Borders	180 sq ft	1.3%
Total Impervious	3,084 sq ft	22.0%
Landscaped Areas	1,539 sq ft	11.0%



ZONING: R2 Single Family Urban
BUILDING SETBACKS
Front Yard - 20 feet
Side Yard, Interior Lot Line - 10 feet
Side Yard, Street - 20 Feet
Rear Yard - 30 feet

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Landmark Surveying, Inc.
Joel T. Anz
Joel T. Anz, Minnesota License No. 13775
February 17, 2015
Date

Landmark Surveying, Inc.
21090 Olinda Trail North, Suite B
P.O. Box 65
Scandia, Minnesota 55073
Office number: 651-433-3421
Cell number: 651-235-8880
E-mail: inthefield@frontiernet.net

CERTIFICATE OF SURVEY

Land Description
Warranty Deed, Doc. No. 3509928

Lots Five (5) and Six (6) in Block
Fifty-three (53) of Bayport,
Washington County, Minnesota.

Lot 5 and Lot 6, Block 53, BAYPORT
(Platted as SOUTH STILLWATER)
City of Bayport, Washington County, Minnesota

Site Plan
Proposed Conditions
Prepared for:

Kyle R. Carlson
207 6th Street North
Bayport, Minnesota 55003

NOTE: See SHEET 1 for additional boundary information.

PID: 10-029-20-14-0008

Legend

- Denotes set 1/2 inch iron pipe, 18 inches long, with a plastic cap inscribed ANEZ LS 13775.
 - Denotes found iron monument as noted.
 - P= Refers to recorded distances on the plat of BAYPORT (platted as SOUTH STILLWATER) on file and of record in the Office of the County Recorder, Washington County, Minnesota.
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- Concrete Surface
 - Bituminous Surface
 - Rock/Stone Border
 - Wood Mulch
 - Coniferous Tree
 - Deciduous Tree
 - Wood Fence

Parcel Area: 13,980 sq. ft.

Proposed Coverage	Area	% of Parcel Area
House and Garage	2,978 sq ft	21.29%
Concrete/Bituminous	1,820 sq ft	13.01%
Deck/Steps	72 sq ft	0.51%
Total Impervious	4,870 sq ft	34.81%
Landscape Borders	130 sq ft	0.93%
Landscaped Areas	880 sq ft	6.29%

ZONING: R2 Single Family Urban

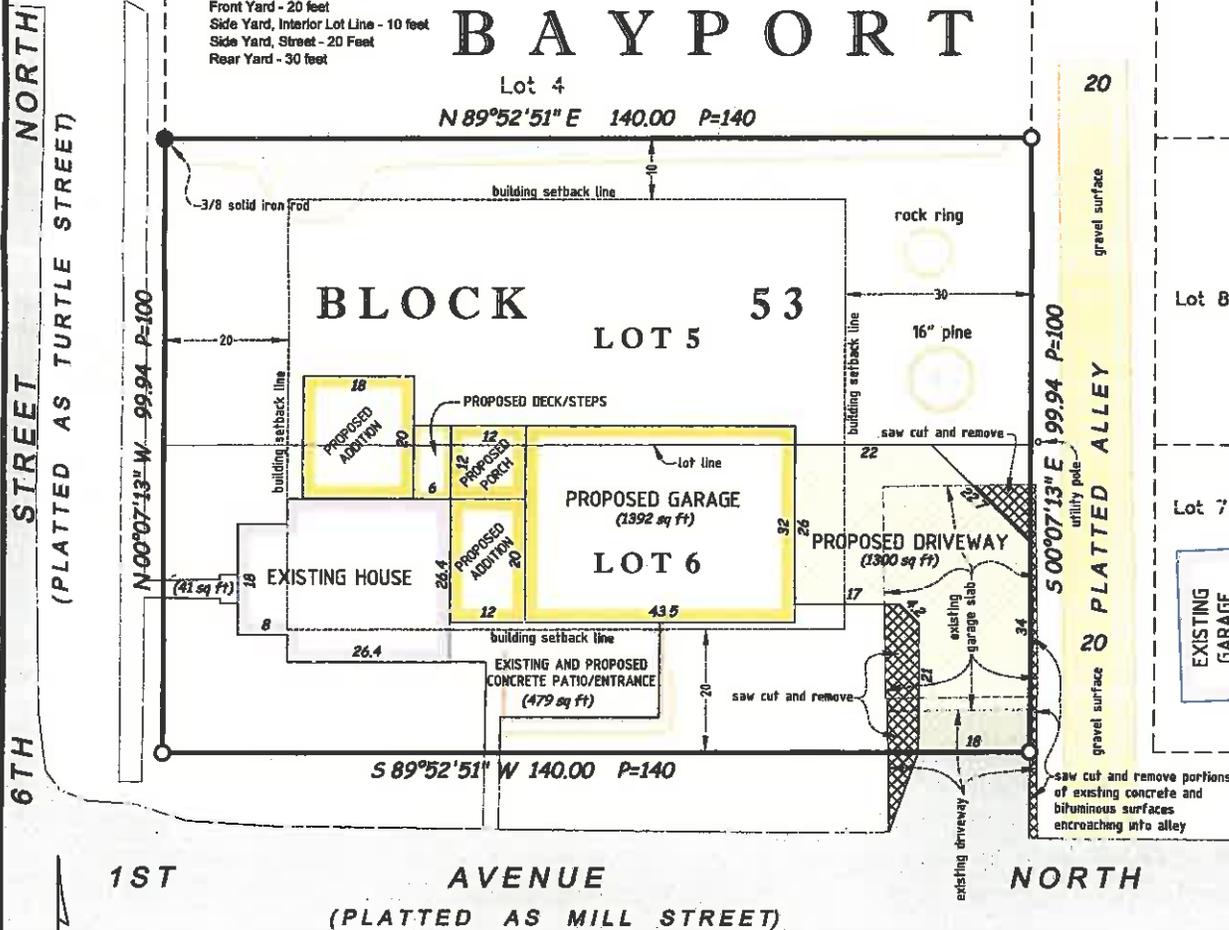
BUILDING SETBACKS

Front Yard - 20 feet
Side Yard, Interior Lot Line - 10 feet
Side Yard, Street - 20 Feet
Rear Yard - 30 feet

BAYPORT

Lot 4

N 89°52'51" E 140.00 P=140



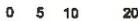
1ST

AVENUE

(PLATTED AS MILL STREET)

NORTH

Scale in Feet



1 inch = 20 feet

OFFICIAL COPIES OF THIS MAP ARE CRIMP SEALED

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Landmark Surveying, Inc.

Joel T. Anez
Joel T. Anez Minnesota License No. 13775

February 17, 2015

Date

Landmark Surveying, Inc.

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